

**TEN TRAILS MPD**

**PHASE 2 PLAT F PRELIMINARY PLAT**

**PLN23-0019**

**EXHIBIT 1**

**TITLE:** Ten Trails Phase 2 Plat F Preliminary Plat Planset

**PREPARED BY:** David Evans and Associates, Inc. on behalf of  
Oakpointe

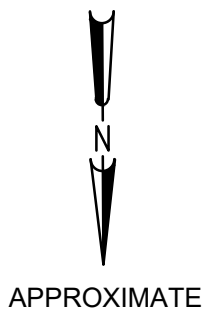
**DATE:** Revision #3 on Title Block dated 11/15/2023



# CITY OF BLACK DIAMOND

## TEN TRAILS - PRELIMINARY PLAT PHASE 2 - PLAT F

### PROJECT NUMBER PLN23-0019



**NOTE**

REFER TO SHEET CV4 FOR BASE DENSITY USED AND TRANSFER OF DEVELOPMENT RIGHTS (TDR) CALCULATIONS.

**SHEET INDEX**

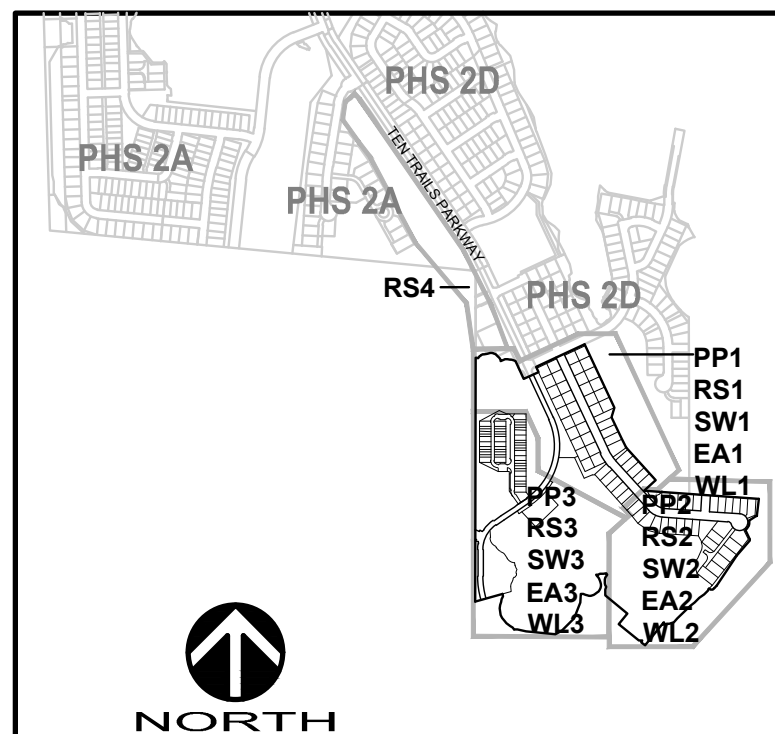
CV0	COVER SHEET
CV1	COVER SHEET
CV2	SITE BOUNDARY EXHIBIT
CV3	TRACT AND OPEN SPACE EXHIBIT
CV4	PARCEL AND DENSITY EXHIBIT
PP1-PP3	PRELIMINARY PLAT PLANS
WL1-WL3	WETLAND BUFFER AVERAGING
UA1	UTILITY AVAILABILITY & CRITICAL AREAS
RS1-RS4	ROAD, STORM DRAINAGE AND GRADING PLANS
RD1	ROAD SECTIONS - ACCESS EXHIBIT
SW1-SW3	SANITARY SEWER AND WATER PLANS
EA1-EA3	EASEMENT PLANS

NOTE: These plans are not approved for filing or construction unless all signature spaces are signed below	
Approved (Mayor)	(date)
Approved (MDRT Director)	(date)
Approved (Planning)	(date)
Approved (Public Works)	(date)
Approved (Master Developer)	(date)



# TEN TRAILS

## PHASE 2 - PLAT F



## SHEET INDEX

## SITE INFORMATION

- |                           |                       |  |
|---------------------------|-----------------------|--|
| GROSS SITE AREA           | 1,928,350 SF/44.27 AC |  |
| GROSS RESIDENTIAL DENSITY | SEE SHEET CV4         |  |
| NET RESIDENTIAL DENSITY   | SEE SHEET CV4         |  |
| DENSITY FROM TDR'S        | 18                    |  |
| LAND USE DESIGNATIONS     | V-PARCEL #<br>31      | DESIGNATION<br>MASTER PLANNED DEVELOPMENT LOW DENSITY<br>RESIDENTIAL (MPD-L) - 1-8 DU/ACRE |
|                           | 32 AND 33             | MASTER PLANNED DEVELOPMENT HIGH DENSITY<br>RESIDENTIAL (MPD-H) - 13-18 DU/ACRE             |

DEVELOPER:	COD BLACK DIAMOND PARTERS LLC 3025 112TH AVENUE NE, SUITE 100 BELLEVUE, WA. 98004 (425) 898-2100 CONTACT: JUSTIN WORTMAN
SURVEYOR/ENGINEER/ PLANNER:	DAVID EVANS AND ASSOCIATES, INC. 20300 WOODINVILLE SNOHOMISH ROAD, NE WOODINVILLE, WA. 98072 (425) 415-2000 CONTACT: BEAU J. WILLERT, P.E.

PARCEL NUMBERS: 2221069035, 2221069041, 2221069042, 2221069052, 2221069053

## NAD '83/91, WASHINGTON NORTH ZONE

## VERTICAL DATUM

NAVD 88

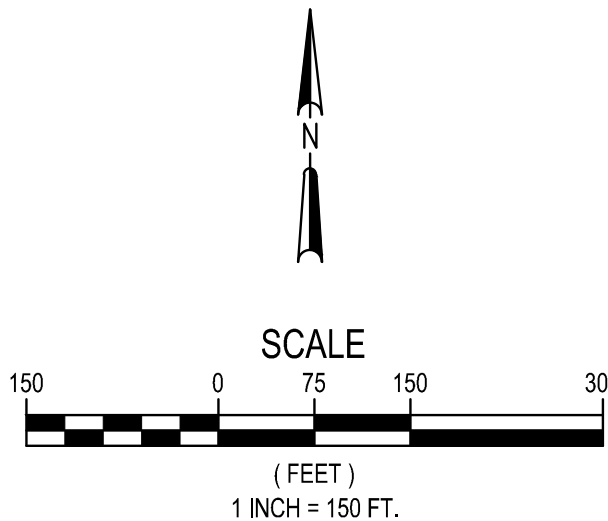
ORIGINAL BENCHMARK - WGS SURVEY DATA WAREHOUSE, SURVEY CONTROL POINT DESIGNATION "Z253", DATABASE POINT ID #43270. BENCHMARK IS A USC & GS BRASS DISK, STAMPED "CGS Z 253 1944". SET IN A DRILL HOLE IN THE CONCRETE BASE OF THE RAILROAD OVERPASS PIER, LOCATED ALONG THE BURLINGTON-NORTHERN RAILROAD LINE, APPROXIMATELY 1/4 MILE SOUTH OF THE INTERSECTION OF SE 28TH STREET AND STATE HIGHWAY 169. THE RAILROAD OVERPASS PIER IS 3 RAILS NORTHWEST OF STATE HIGHWAY 169 AND THE BRASS DISK IS APPROXIMATELY 5.9 FEET HIGHER THAN THE RAILROAD TRACK.

ELEVATION = 568.227 FEET

NORTH 84°32'13" WEST, BEING THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 22,  
TOWNSHIP 21 NORTH, RANGE 6 EAST, W.M.

THE PROPERTY BOUNDARIES SHOWN OUTSIDE OF THE PRELIMINARY PLAT BOUNDARY HAVE BEEN COMPILED FROM THE KING COUNTY ASSESSOR'S MAPS FOR SECTION 15 AND THE NORTHWEST AND NORTHEAST QUARTERS OF SECTION 22, ALL IN TOWNSHIP 21 NORTH, RANGE 6 EAST, W.M. THEY ARE SHOWN HEREON TO AID IN THE INTERPRETATION OF THE MAP.

THE CONTOURS WEST OF THE PRELIMINARY PLAT BOUNDARY WERE INTERPOLATED FROM PUBLIC DOMAIN AIRBORNE LIDAR DATA ACQUIRED IN 2003 BY THE PUGET SOUND LIDAR CONSORTIUM (PSLC) AND BLENDED WITH THE ONSITE GROUND TOPOGRAPHIC SURVEY. LIDAR HAS DEMONSTRATED ADVANTAGES IN DEFINING GROUND SURFACES IN SHADOWED AREAS BUT MAY NOT ADEQUATELY DEFINE THE GROUND IN OBSCURED AREAS. THE ACCURACY OF THE CONTOURS AND DIGITAL TERRAIN MODEL IS CONTINGENT ON THE ACCURACY OF THE PSLC DATA. IT IS NOT EXPECTED THAT THESE CONTOURS CONFORM TO NATIONAL STANDARDS FOR SPATIAL DATA ACCURACY. THIS DATA IS INTENDED FOR PRELIMINARY SITE EVALUATION ONLY.



**DAVID EVANS  
AND ASSOCIATES INC**  
20300 Woodinville Snohomish Rd NE  
Woodinville Washington 98072  
Phone: 425.415.2000

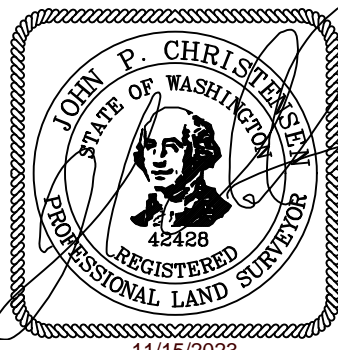
# PRELIMINARY PLAT COVER SHEET

## TEN TRAILS PHASE 2 - PLATF

CITY OF BLACK DIAMOND

NO	DATE	REVISION	REVIEWED BY:	DATE:	BY CK
1	8/21/23	CLIENT REVISIONS			DAMTFM
2	9/6/23	OWNER/CLIENT REVISIONS			DAMTFM
3	11/15/23	PER CITY COMMENT DATED 11/14/23			DL BUW

## PRELIMINARY



CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
**OAKPCBDP6001**

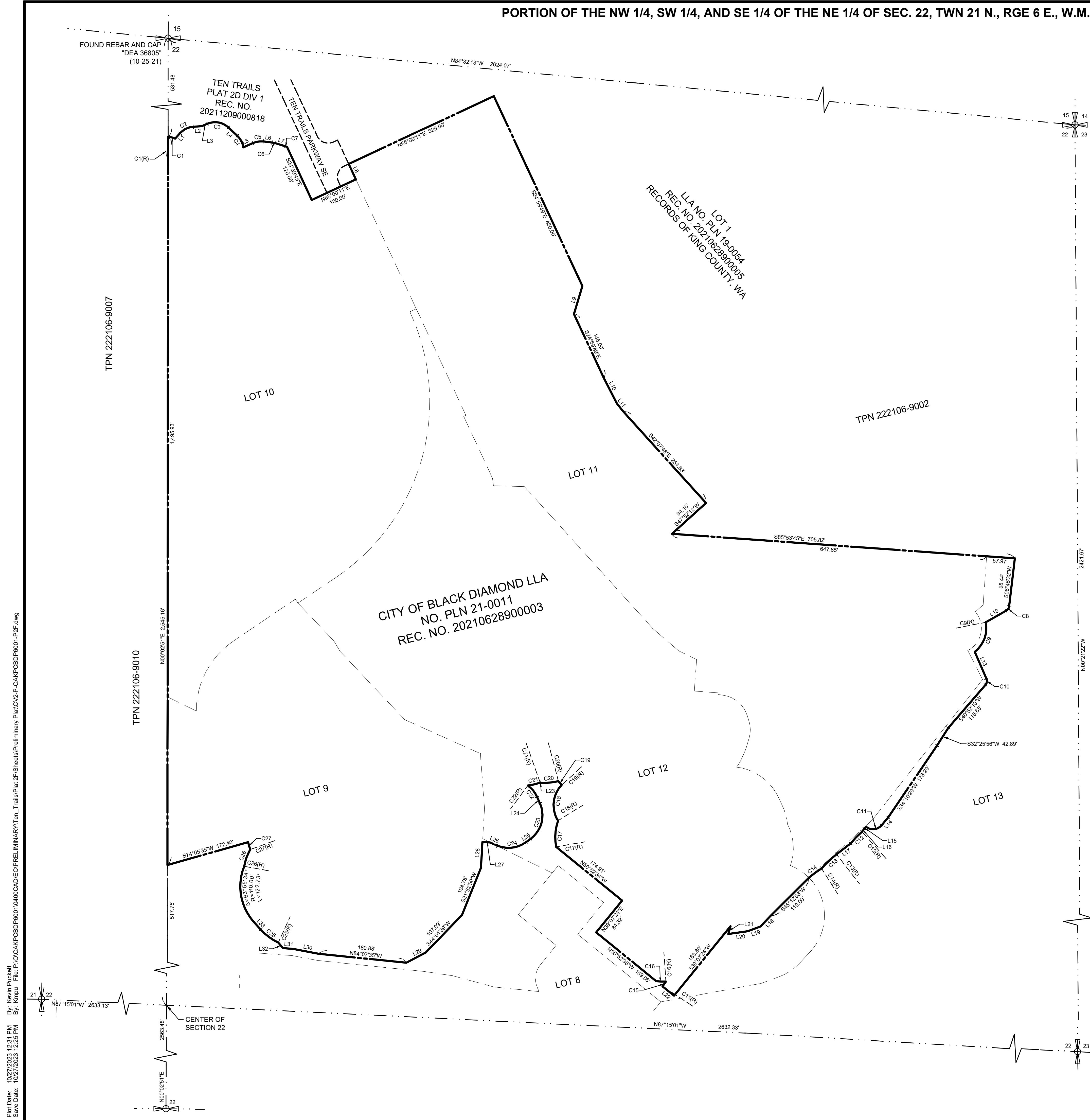
SHEET NO.

# CV1

Plot Date: 11/16/2023 6:00 AM  
By: Denise Liffin  
Save Date: 11/16/2023 5:59 AM  
By: Dexl File: P:\OAKPCBDP6001\0400CADIECIPRELIMINARY\Ten\_Trails\Plat 2F\Sheets\Preliminary Plat\CV1-P-OAKPCBDP6001-P2F.dwg



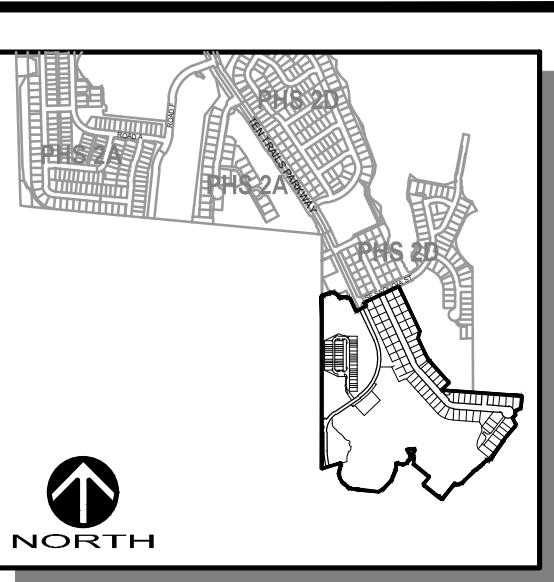
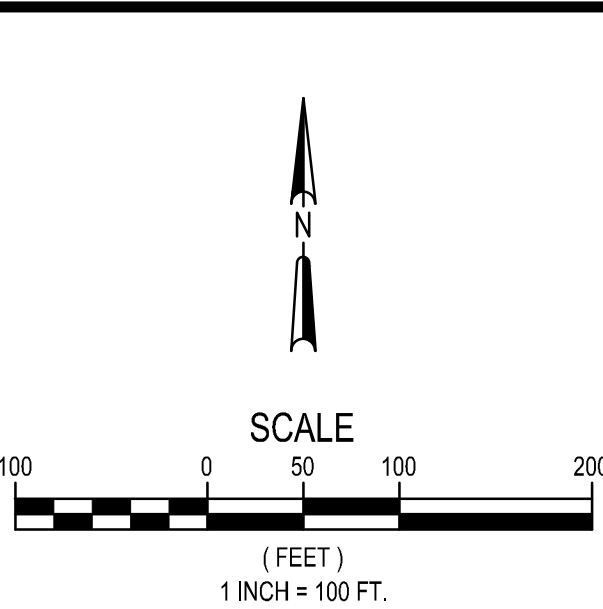
PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.



LINE TABLE		
LINE	BEARING	LENGTH
L1	N41°52'15"E	15.18'
L2	N86°07'43"E	17.62'
L3	N66°41'37"E	12.46'
L4	S46°55'36"E	25.05'
L5	N65°56'09"E	21.76'
L6	S82°56'37"E	18.12'
L8	N24°59'49"W	35.00'
L9	S16°38'12"W	60.21'
L10	S27°18'46"E	58.35'
L11	S37°44'16"E	20.14'
L12	S60°15'45"W	47.95'
L13	S23°22'24"E	60.27'
L14	S40°04'11"W	21.37'
L15	S38°25'05"W	5.06'
L16	S38°18'22"W	7.99'
L17	S52°10'00"W	36.46'
L18	S43°19'11"W	31.46'
L19	S69°50'01"W	27.93'
L20	S81°49'05"W	40.27'
L21	N15°49'12"E	16.58'
L22	N50°52'36"W	30.25'
L23	N85°43'26"W	1.05'
L24	S24°24'05"E	14.02'
L25	S53°27'56"W	11.85'
L26	N66°58'55"W	16.12'
L27	N86°36'31"W	15.53'
L28	S03°23'29"W	52.86'
L29	S62°16'00"W	45.12'
L30	N79°06'25"W	53.38'
L31	N85°45'41"W	21.05'
L32	N39°42'54"W	14.80'
L33	N41°55'05"W	20.76'

CURVE TABLE			
CURVE	DELTA	RADIUS	LENGTH
C1	021°44'58"	40.00'	15.18'
C2	044°15'28"	40.00'	30.90'
C3	066°22'47"	40.00'	46.34'
C4	039°05'35"	40.00'	27.29'
C5	031°07'14"	40.00'	21.73'
C6	009°44'48"	40.00'	6.80'
C7	007°13'43"	40.00'	5.05'
C8	053°30'13"	13.00'	12.14'
C9	063°26'56"	61.50'	68.10'
C10	064°14'34"	12.00'	13.46'
C11	088°59'12"	23.13'	35.92'
C12	008°19'48"	231.16'	33.61'
C13	010°45'30"	225.00'	42.25'
C14	006°11'54"	225.00'	32.19'
C15	003°07'47"	225.00'	12.29'
C16	010°23'38"	110.00'	19.95'
C17	028°14'06"	110.00'	54.21'
C18	075°53'50"	60.00'	79.48'
C19	005°29'10"	110.00'	10.53'
C20	019°02'22"	110.00'	36.55'
C21	013°40'42"	110.00'	26.26'
C22	029°17'46"	60.00'	30.68'
C23	077°52'01"	60.00'	81.54'
C24	059°33'09"	60.00'	62.38'
C25	023°32'22"	110.00'	45.19'
C26	019°08'23"	110.00'	36.75'
C27	006°15'09"	110.00'	15.84'

RADIAL BEARING TABLE	
CURVE #	RADIAL BEARING
C1	S04°08'19"W
C9	S78°45'57"W
C12	S41°46'27"E
C13	S37°45'58"E
C14	S32°16'38"E
C15	S56°53'43"E
C16	N04°00'45"W
C17	N80°14'34"E
C18	N57°50'35"E
C19	N47°42'07"E
C20	N14°45'49"W
C21	N19°35'07"W
C22	S36°18'09"W
C25	N24°32'33"E
C26	S79°32'07"E
C27	N70°08'18"E



LEGAL DESCRIPTION

THOSE PORTIONS OF LOTS 9 THROUGH 13, INCLUSIVE, OF CITY OF BLACK DIAMOND LOT LINE ADJUSTMENT NO. PLN 21-0011, FILED IN VOLUME 448 OF SURVEYS, AT PAGE 111 AND RECORDED UNDER RECORDING NO. 2021062890003, RECORDS OF KING COUNTY, WASHINGTON, LYING WITHIN THE NORTHWEST, SOUTHWEST AND SOUTHEAST QUARTERS OF THE NORTHEAST QUARTER OF SECTION 22, TOWNSHIP 21 NORTH, RANGE 6 EAST, WILLAMETTE MERIDIAN, AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTH QUARTER CORNER OF SAID SECTION 22;  
THENCE SOUTHERLY ALONG THE WEST LINE OF THE NORTHEAST QUARTER OF SAID SECTION 22, SOUTH 00°02'51" WEST, 531.48 FEET TO THE NORTH LINE OF SAID LOT 10 AND THE TRUE POINT OF BEGINNING;  
THENCE EASTERLY ALONG SAID NORTH LINE AND LEAVING SAID WEST LINE, ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS SOUTH 04°08'19" WEST, HAVING A RADIUS OF 40.00 FEET, THROUGH A CENTRAL ANGLE OF 21°44'58", AN ARC DISTANCE OF 15.18 FEET;  
THENCE NORTHEASTERLY, NORTH 41°52'15" EAST, 15.18 FEET;  
THENCE EASTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 40.00 FEET, THROUGH A CENTRAL ANGLE OF 44°15'28", AN ARC DISTANCE OF 30.90 FEET;  
THENCE CONTINUING EASTERLY, NORTH 86°07'43" EAST, 17.62 FEET;  
THENCE NORTHEASTERLY, NORTH 66°41'37" EAST, 12.46 FEET;  
THENCE SOUTHEASTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 40.00 FEET, THROUGH A CENTRAL ANGLE OF 66°22'47", AN ARC DISTANCE OF 46.34 FEET;  
THENCE CONTINUING SOUTHEASTERLY, SOUTH 46°55'36" EAST, 25.05 FEET;  
THENCE SOUTHERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 40.00 FEET, THROUGH A CENTRAL ANGLE OF 39°05'35", AN ARC DISTANCE OF 27.29 FEET;  
THENCE NORTHEASTERLY, NORTH 65°56'09" EAST, 21.76 FEET;  
THENCE EASTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 40.00 FEET, THROUGH A CENTRAL ANGLE OF 31°07'14", AN ARC DISTANCE OF 21.73 FEET;  
THENCE SOUTHERLY, SOUTH 82°56'37" EAST, 18.12 FEET;  
THENCE SOUTHEASTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 40.00 FEET, THROUGH A CENTRAL ANGLE OF 09°44'48", AN ARC DISTANCE OF 6.80 FEET;  
THENCE EASTERLY, SOUTH 73°11'49" EAST, 30.24 FEET;  
THENCE SOUTHEASTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 40.00 FEET, THROUGH A CENTRAL ANGLE OF 07°13'43", AN ARC DISTANCE OF 5.05 FEET;  
THENCE CONTINUING SOUTHEASTERLY, SOUTH 24°59'49" EAST, 35.00 FEET;  
THENCE NORTHEASTERLY, NORTH 65°00'11" EAST, 100.00 FEET TO THE SOUTHWESTERLY LINE OF SAID LOT 11;  
THENCE NORTHWESTERLY ALONG SAID SOUTHWESTERLY LINE, NORTH 24°59'49" WEST, 35.00 FEET;  
THENCE NORTHEASTERLY AND LEAVING SAID SOUTHWESTERLY LINE, NORTH 65°00'11" EAST, 329.00 FEET TO THE NORTHEASTERLY LINE OF SAID LOT 11;  
THENCE SOUTHEASTERLY ALONG SAID NORTHEASTERLY LINE, SOUTH 24°59'49" EAST, 430.00 FEET;  
THENCE SOUTHERLY, SOUTH 16°38'12" WEST, 60.21 FEET;  
THENCE SOUTHEASTERLY, SOUTH 24°59'49" EAST, 145.00 FEET;  
THENCE CONTINUING SOUTHEASTERLY, SOUTH 27°18'46" EAST, 58.35 FEET;  
THENCE SOUTH 37°44'16" EAST, 20.14 FEET;  
THENCE SOUTH 42°07'48" EAST, 254.83 FEET;  
THENCE SOUTHWESTERLY, SOUTH 47°52'12" WEST, 94.16 FEET;  
THENCE EASTERLY, SOUTH 85°53'45" EAST, 647.85 FEET TO THE NORTHWEST CORNER OF SAID LOT 13;  
THENCE CONTINUING EASTERLY ALONG THE NORTH LINE OF SAID LOT 13, SOUTH 85°53'45" EAST, 57.97 FEET;  
THENCE SOUTHERLY AND LEAVING SAID NORTH LINE, SOUTH 06°45'02" WEST, 98.44 FEET;  
THENCE SOUTHWESTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 13.00 FEET, THROUGH A CENTRAL ANGLE OF 05°30'13", AN ARC DISTANCE OF 12.14 FEET;  
THENCE SOUTH 60°15'45" WEST, 47.95 FEET;  
THENCE SOUTHERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS SOUTH 79°45'57" WEST, HAVING A RADIUS OF 61.50 FEET, THROUGH A CENTRAL ANGLE OF 63°26'56", AN ARC DISTANCE OF 68.10 FEET;  
THENCE SOUTHEASTERLY, SOUTH 23°22'24" EAST, 60.27 FEET;  
THENCE SOUTHWESTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 12.00 FEET, THROUGH A CENTRAL ANGLE OF 64°14'34", AN ARC DISTANCE OF 13.46 FEET;  
THENCE SOUTH 40°52'10" WEST, 116.65 FEET;  
THENCE SOUTH 32°25'56" WEST, 42.89 FEET;  
THENCE SOUTH 34°10'29" WEST, 178.29 FEET;  
THENCE SOUTH 40°04'11" WEST, 21.37 FEET;  
THENCE WESTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 23.13 FEET, THROUGH A CENTRAL ANGLE OF 88°59'12", AN ARC DISTANCE OF 35.92 FEET;  
THENCE SOUTHWESTERLY, SOUTH 38°20'08" WEST, 5.06 FEET;  
THENCE SOUTH 38°18'22" WEST, 7.99 FEET;  
THENCE ALONG A NON-TANGENT CURVE TO THE LEFT, THE CENTER OF WHICH BEARS SOUTH 41°46'27" EAST, HAVING A RADIUS OF 13.00 FEET, THROUGH A CENTRAL ANGLE OF 08°19'48", AN ARC DISTANCE OF 33.61 FEET;  
THENCE CONTINUING SOUTHWESTERLY, SOUTH 52°10'00" WEST, 36.46 FEET;  
THENCE ALONG A NON-TANGENT CURVE TO THE LEFT, THE CENTER OF WHICH BEARS SOUTH 37°45'58" EAST, HAVING A RADIUS OF 225.00 FEET, THROUGH A CENTRAL ANGLE OF 10°45'30", AN ARC DISTANCE OF 42.25 FEET;  
THENCE CONTINUING SOUTHWESTERLY ALONG A NON-TANGENT CURVE TO THE LEFT, THE CENTER OF WHICH BEARS SOUTH 32°16'38" EAST, HAVING A RADIUS OF 225.00 FEET, THROUGH A CENTRAL ANGLE OF 08°11'54", AN ARC DISTANCE OF 32.19 FEET;  
THENCE SOUTHWESTERLY, SOUTH 39°07'24" WEST, 183.80 FEET;  
THENCE NORTHEASTERLY, NORTH 60°52'36" WEST, 30.25 FEET;  
THENCE NORTHEASTERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS SOUTH 56°53'43" EAST, HAVING A RADIUS OF 225.00 FEET, THROUGH A CENTRAL ANGLE OF 03°07'47", AN ARC DISTANCE OF 12.29 FEET;  
THENCE WESTERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 04°00'45" WEST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 10°23'38", AN ARC DISTANCE OF 19.95 FEET;  
THENCE NORTHWESTERLY, NORTH 50°52'36" WEST, 159.08 FEET;  
THENCE NORTHEASTERLY, NORTH 39°07'24" EAST, 84.32 FEET;  
THENCE NORTHWESTERLY, NORTH 50°52'36" WEST, 174.91 FEET;  
THENCE NORTHERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 80°14'34" EAST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 28°14'06", AN ARC DISTANCE OF 34.21 FEET;  
THENCE NORTHEASTERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 57°50'35" EAST, HAVING A RADIUS OF 60.00 FEET, THROUGH A CENTRAL ANGLE OF 75°53'50", AN ARC DISTANCE OF 79.48 FEET;  
THENCE WESTERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 47°42'07" EAST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 05°29'10", AN ARC DISTANCE OF 10.53 FEET;  
THENCE WESTERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 14°45'49" WEST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 19°02'22", AN ARC DISTANCE OF 36.55 FEET;  
THENCE CONTINUING WESTERLY, NORTH 85°43'26" WEST, 1.05 FEET;  
THENCE ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 19°35'07" WEST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 13°40'42", AN ARC DISTANCE OF 26.26 FEET;  
THENCE SOUTHERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS SOUTH 36°18'09" WEST, HAVING A RADIUS OF 60.00 FEET, THROUGH A CENTRAL ANGLE OF 29°17'46", AN ARC DISTANCE OF 30.68 FEET;  
THENCE SOUTHEASTERLY, SOUTH 24°24'05" EAST, 14.02 FEET;  
THENCE SOUTHWESTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 60.00 FEET, THROUGH A CENTRAL ANGLE OF 77°52'01", AN ARC DISTANCE OF 81.54 FEET;  
THENCE CONTINUING SOUTHWESTERLY, SOUTH 53°27'56" WEST, 11.85 FEET;  
THENCE WESTERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 60.00 FEET, THROUGH A CENTRAL ANGLE OF 59°33'09", AN ARC DISTANCE OF 62.38 FEET;  
THENCE NORTHWESTERLY, NORTH 66°58'55" WEST, 16.12 FEET;  
THENCE WESTERLY, NORTH 86°36'31" WEST, 15.53 FEET;  
THENCE SOUTHWESTERLY, SOUTH 03°23'29" WEST, 52.86 FEET;  
THENCE SOUTH 44°01'39" WEST, 107.09 FEET;  
THENCE SOUTH 62°16'00" WEST, 45.12 FEET;  
THENCE WESTERLY, NORTH 84°07'35" WEST, 180.88 FEET;  
THENCE CONTINUING WESTERLY, NORTH 79°06'25" WEST, 53.38 FEET;  
THENCE NORTH 85°45'41" WEST, 21.05 FEET;  
THENCE NORTH 39°42'54" WEST, 14.80 FEET;  
THENCE NORTHERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 24°32'33" EAST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 23°32'22", AN ARC DISTANCE OF 45.19 FEET;  
THENCE NORTHWESTERLY, NORTH 41°55'05" WEST, 20.76 FEET;  
THENCE NORTHERLY ALONG A CURVE TO THE RIGHT, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 63°55'34", AN ARC DISTANCE OF 122.73 FEET;  
THENCE NORTHERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS SOUTH 79°32'07" EAST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 19°08'23", AN ARC DISTANCE OF 36.75 FEET;  
THENCE NORTHERLY ALONG A NON-TANGENT CURVE TO THE RIGHT, THE CENTER OF WHICH BEARS NORTH 70°08'18" EAST, HAVING A RADIUS OF 110.00 FEET, THROUGH A CENTRAL ANGLE OF 08°15'09", AN ARC DISTANCE OF 15.84 FEET;  
THENCE WESTERLY, SOUTH 74°05'35" WEST, 172.40 FEET TO THE WEST LINE OF SAID NORTHEAST QUARTER;  
THENCE NORTHERLY ALONG SAID WEST LINE, NORTH 00°02'51" EAST, 1,495.93 FEET TO THE TRUE POINT OF BEGINNING.

SITUATE IN THE CITY OF BLACK DIAMOND, KING COUNTY, WASHINGTON.

CONTAINING A TOTAL AREA OF 1,928,350 SF OR 44.27 AC



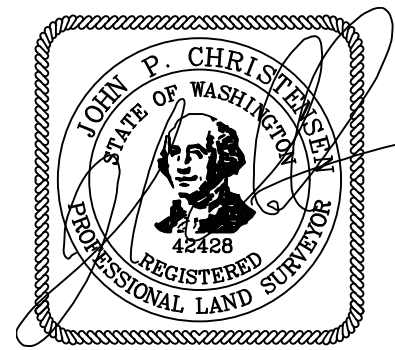
PRELIMINARY PLAT  
SITE BOUNDARY EXHIBIT

TEN TRAILS  
PHASE 2 - PLAT F

CITY OF BLACK DIAMOND

DATE:	BY:	CHK:
10/27/2023	DAVID EVANS	DAVID EVANS
10/27/2023	DAVID EVANS	DAVID EVANS
10/27/2023	DAVID EVANS	DAVID EVANS

PRELIMINARY



CHECKED BY: MMH  
DESIGNED BY:  
DRAWN BY: ARJ  
FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.

CV2



# TEN TRAILS PHASE 2 - PLAT F

PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.

## OPEN SPACE TRACT TABLE\*

TRACT	USE**	SIZE (SF)	PARK TYPE	OWNERSHIP	MAINTENANCE	CONSERVATION DEED CATEGORY PER SECTION 2.4
TRACT 901	LA/UT	1,350		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 902	UT/LA/PA	23,994		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 903	LA/PA/UT	7,731		T.T.R.O.A.	T.T.R.O.A.	v
TRACT 905	LA/UT	4,235		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 906	SA	2,161		T.T.R.O.A.	T.T.R.O.A.	iii
TRACT 907	LA/PA	1,970		T.T.R.O.A.	T.T.R.O.A.	v
TRACT 908	LA/UT	1,400		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 909	PK/LA/UT/PA	21,592	NEIGHBORHOOD PARK	T.T.R.O.A.	T.T.R.O.A.	v
TRACT 910	UT/LA/PA	4,394		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 912	LA/UT	1,961		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 913	LA/PA/UT	2,033		T.T.R.O.A.	T.T.R.O.A.	v
TRACT 914	LA/PA	3,185		T.T.R.O.A.	T.T.R.O.A.	v
TRACT 915	LA/UT	9,237		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 916	SA/UT/PA	266,006		T.T.R.O.A.	T.T.R.O.A.	iii
TRACT 917	UT/LA/PA	3,204		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 918	LA/UT/PA	5,968		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 919	UT/LA/PA/SA	49,822		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 920	SA/UT/PA	833,293		T.T.R.O.A.	T.T.R.O.A.	iii
TRACT 921	UT/LA/SA	8,831		T.T.R.O.A.	T.T.R.O.A.	vii
TRACT 922	PK/LA/UT	7,987	POCKET PARK	T.T.R.O.A.	T.T.R.O.A.	v

## OPEN SPACE TRACT TABLE NOTES

TOTAL OPEN SPACE AREA = 1,260,355 SF/28.93 AC (SEE NOTE BELOW REGARDING AREAS HIGHLIGHTED IN THE OPEN SPACE TRACT TABLE)

PERMANENT PUBLIC ACCESS EASEMENTS CONSISTENT WITH SECTION 9.9.3 OF THE VILLAGES MPD DEVELOPMENT AGREEMENT SHALL BE PROVIDED FOR ALL PARKS AND TRAILS ON THE FINAL PLAT.

\*OPEN SPACE, AS DEFINED IN SECTION 14 OF THE VILLAGES MPD DEVELOPMENT AGREEMENT IS COMPRISED OF LA (LANDSCAPE), PA (PEDESTRIAN ACCESS), PK (PARK), NL (NATURAL LANDSCAPE) AND SA (SENSITIVE AREA AND BUFFER).

\*\*THESE ARE PROPOSED PRIMARY USES, BUT DOES NOT PRECLUDE UTILITIES AND ACCESS FROM BEING INSTALLED WHERE APPROPRIATE IN COMPLIANCE WITH THE SENSITIVE AREAS ORDINANCE AS FOUND IN EXHIBIT E OF THE VILLAGES MPD DEVELOPMENT AGREEMENT.

HIGHLIGHTED TRACTS IN THE NET OPEN SPACE TRACT TABLE REPRESENT THE AREAS THAT ARE COUNTED IN THE OPEN SPACE TABLES ON THIS SHEET. THESE TRACTS (902, 910, 912 AND 916- 920) EXCLUDE A 4.5' WIDTH OF LANDSCAPE FROM THE OPEN SPACE TOTAL ALONG TEN TRAILS PARKWAY (14,073 SF TOTAL EXCLUDED)

## OPEN SPACE TABLE

PHASE 2 PLAT F	GROSS ACRES	BDUGAA/OPEN SPACE REQUIREMENT	MPD REQUIREMENT (IF APPLICABLE)	REQUIRED OPEN SPACE	OPEN SPACE PREVIOUSLY RECORDED	REQUIRED OPEN SPACE LESS PREVIOUSLY RECORDED PLATS	OPEN SPACE PROVIDED IN THIS PLAT	REMAINING OPEN SPACE REQUIRED
WEST ANNEXATION AREA PARCEL C PARCEL D PARCEL G	54.62 225.99 8.06	63.30	0	63.3	21.59 (PASSIVE) 16.58 (ACTIVE) (PP1A, DIV.1) (PH2, PLAT C, DIV. 1) (PP1A, DIV. 2) (V-13) (P1A DIVS. 3-5 AND 8) (PH2, PLAT C, DIV. 2) (V-24) (PLAT 2A, DIVS 1-3) (PLAT 2A, DIV 4) (PP1A, DIV. 6) (PLAT 2A, DIVS. 5) (PLAT 2E) (PLAT 2D, DIV.1-2) (PHASE 1B PLAT A)	25.13	0 (PASSIVE) 0 (ACTIVE)	25.13
PARCEL E	151.15	0	75.58	75.58	95.73 (PASSIVE) 2.29 (ACTIVE) (PH2, PLAT C, DIV. 1) (PH2, PLAT C, DIV. 2) (PLAT 2E) (PLAT 2D, DIV.1-2) (PLAT 2D, DIV. 3-4)	0	0 (PASSIVE) 0 (ACTIVE)	0
PARCEL BDA	395.74	0	197.87	197.87	10.04 (PASSIVE) 0.95 (ACTIVE) (PLAT 2D, DIV.1-2) Plat 2D, Div. 3-4	186.88	28.25 (PASSIVE) .68 (ACTIVE)	157.95
TOTAL IN CITY/UGA MPD OPEN SPACE	1,196.40	145	336.4	481.4	127.36 (PASSIVE) 19.82 (ACTIVE)	334.22	28.25 (PASSIVE) .68 (ACTIVE)	305.29

## MPD OPEN SPACE TABLE

	OPEN SPACE DEDICATED	PERCENTAGE OF BUILD OUT COMPLETED
OVERALL MPD TO DATE (RECORDED PLATS)	147.18	36%
OVERALL MPD UPON PROJECT APPROVAL OF PHASE 2 PLAT F	176.11	40%
WEST ANNEXATION AREA (PARCEL C,D,G) TO DATE (RECORDED PLATS)	38.17	89%
WEST ANNEXATION AREA (PARCEL C,D,G) UPON PROJECT APPROVAL OF PHASE 2 PLAT F	38.17	89%
PARCEL E TO DATE (RECORDED PLATS)	98.02	100%
PARCEL E UPON PROJECT APPROVAL OF PHASE 2 PLAT F	98.02	100%
PARCEL BDA TO DATE (RECORDED PLATS)	10.99	6%
PARCEL BDA UPON PROJECT APPROVAL OF PHASE 2 PLAT F	39.92	17%

## TRACT TABLE

TRACT	USE**	SIZE (SF)	OWNERSHIP	MAINTENANCE
TRACT 901	LA/UT	1,350	T.T.R.O.A.	T.T.R.O.A.
TRACT 902	UT/LA/PA	26,442	T.T.R.O.A.	T.T.R.O.A.
TRACT 903	LA/PA/UT	7,731	T.T.R.O.A.	T.T.R.O.A.
TRACT 904	AC/UT/PA	7,377	T.T.R.O.A.	T.T.R.O.A.
TRACT 905	LA/UT	4,235	T.T.R.O.A.	T.T.R.O.A.
TRACT 906	SA	2,161	T.T.R.O.A.	T.T.R.O.A.
TRACT 907	LA/PA	1,970	T.T.R.O.A.	T.T.R.O.A.
TRACT 908	LA/UT	1,400	T.T.R.O.A.	T.T.R.O.A.
TRACT 909	PK/LA/UT/PA	21,592	T.T.R.O.A.	T.T.R.O.A.
TRACT 910	UT/LA/PA	5,645	T.T.R.O.A.	T.T.R.O.A.
TRACT 911	AC/UT/PA	25,532	T.T.R.O.A.	T.T.R.O.A.
TRACT 912	LA/UT	1,988	T.T.R.O.A.	T.T.R.O.A.
TRACT 913	LA/PA/UT	2,033	T.T.R.O.A.	T.T.R.O.A.
TRACT 914	LA/PA	3,185	T.T.R.O.A.	T.T.R.O.A.
TRACT 915	LA/UT	9,237	T.T.R.O.A.	T.T.R.O.A.
TRACT 916	SA/UT/PA	270,920	T.T.R.O.A.	T.T.R.O.A.
TRACT 917	UT/LA/PA	3,488	T.T.R.O.A.	T.T.R.O.A.
TRACT 918	LA/UT/PA	7,579	T.T.R.O.A.	T.T.R.O.A.
TRACT 919	UT/LA/PA/SA	51,735	T.T.R.O.A.	T.T.R.O.A.
TRACT 920	SA/UT/PA	834,918	T.T.R.O.A.	T.T.R.O.A.
TRACT 921	UT/LA/SA	8,831	T.T.R.O.A.	T.T.R.O.A.
TRACT 922	PK/LA/UT	7,987	T.T.R.O.A.	T.T.R.O.A.

TOTAL AREA IN TRACTS - 1,307,338 SF/30.01 AC

## IN-CITY OPEN SPACE IDENTIFICATION PURSUANT TO CONSERVATION EASEMENT DEED

IN ACCORDANCE WITH RECITAL 2.1 AND PARAGRAPH 4 OF EXHIBIT B OF THE CONSERVATION EASEMENT DEED BY AND BETWEEN BD VILLAGE PARTNERS, LP AND THE CITY OF BLACK DIAMOND RECORDED ON JANUARY 6, 2010 UNDER KING COUNTY RECORDING NO. 20100106000723 (THE "CONSERVATION EASEMENT DEED"), CCD BLACK DIAMOND PARTNERS LLC PROPOSES, FOR THE AREA COVERED BY THIS PRELIMINARY PLAT APPLICATION, TO AMEND THE BOUNDARIES OF THE IN-CITY OPEN SPACE, AS DEFINED IN THE CONSERVATION EASEMENT DEED, TO INCLUDE ONLY TRACTS 901-903, 905-910 AND 912-922.

TRACTS 903, 907, 909, 913, 914 AND 922 ARE LANDSCAPED AREAS CONSISTING OF 44,499 SQUARE FEET. SAID TRACTS PRESERVE AND PROTECT THE CONSERVATION VALUES, AS DEFINED IN THE CONSERVATION EASEMENT DEED, IDENTIFIED IN RECITAL 2.4 BY ENHANCING RECREATION OPPORTUNITIES (RECITAL 2.4(V)).

TRACTS 901, 902, 905, 908, 910, 912, 915, 917-919 AND 921 ARE LANDSCAPED AREAS CONSISTING OF 114,396 SQUARE FEET. SAID TRACT PRESERVES AND PROTECTS THE CONSERVATION VALUES, AS DEFINED IN THE CONSERVATION EASEMENT DEED, IDENTIFIED IN RECITAL 2.4 BY PRESERVING VISUAL QUALITY ALONG HIGHWAY, ROAD, AND STREET CORRIDORS OR SCENIC VISTAS (RECITAL 2.4(VII)).

TRACTS 906, 916 AND 920 ARE SENSITIVE AREA TRACTS CONSISTING OF 1,101,460 SQUARE FEET. SAID TRACTS PRESERVE AND PROTECT THE CONSERVATION VALUES, AS DEFINED IN THE CONSERVATION EASEMENT DEED, IDENTIFIED IN RECITAL 2.4 BY CONSERVING AND ENHANCING NATURAL OR SCENIC RESOURCES AND PROMOTING CONSERVATION OF SOILS AND WETLANDS (RECITALS 2.4(I) AND (III)).

## LEGEND

- 2F TRAIL  
--- 2D TRAIL (EXISTING)
- OPEN SPACE\* TRACT/LOT DESIGNATIONS  
LA - LANDSCAPE  
PA - PEDESTRIAN ACCESS  
PK - PARK  
SA - SENSITIVE AREA
- OTHER TRACT DESIGNATIONS  
AC - ACCESS  
FD - FUTURE DEVELOPMENT  
UT - UTILITY
- T.T.R.O.A TEN TRAILS RESIDENTIAL OWNERS ASSOCIATION

Plot Date: 10/27/2023 12:33 PM By: Kevin Puckett  
Save Date: 10/27/2023 11:52 AM By: kmpu Plot: P:\OAKPCBDP6001\0400CAD\CDP\PRELIMINARY\Ten\_Trails\Plat\_2F\Sheets\Preliminary Plat\CV1-P-OAKPCBDP6001-P2F.dwg

## PRELIMINARY



# TEN TRAILS

## PHASE 2 - PLAT F

### LAND USE CAPACITY TABLE

	GROSS DEVELOPMENT PARCEL AREA (ACRES)	NET DEVELOPMENT PARCEL AREA (ACRES)	MPD ZONING	RANGE OF RESIDENTIAL UNITS FOR PARCEL	SQUARE FEET RANGE FOR COMMERCIAL / OFFICE / RETAIL PARCEL	MULTI FAMILY RESIDENTIAL (UNITS)	SINGLEFAMILY RESIDENTIAL (UNITS)	COMMERCIAL / OFFICE / RETAIL SQUARE FOOTAGE	GROSS RESIDENTIAL PARCEL DENSITY	NET RESIDENTIAL PARCEL DENSITY
AUTHORIZED BY THE VILLAGES MPD PERMIT AND DEVELOPMENT AGREEMENT						1200	3600	775,000		
PREVIOUS PLATS/PROJECTS										
PHASE 1A						245	455	190,000		
PHASE 2 PLAT A						0	211	0		
PHASE 2 PLAT C						0	201	0		
PHASE 2 PLAT E						0	43	0		
PHASE 2 PLAT 2 D						0	232	0		
PHASE 1B PLAT A						25	233	205,000		
TOTAL PREVIOUS PLATS/PROJECTS						270	1375	395,000		
CURRENTLY REMAINING VILLAGES MPD						930	2,225	380,000		
PHASE 2 PLAT F										
V-31 (MPD-L 4-8 DU/AC)	12.11	9.65	MPD-L	48-96		0	77	0	6.36	7.98
V-32 (MPD-H 13-18 DU/AC)	1.76	1.43	MPD-H	23-32		32	0	0	18.18	22.38
V-33 (MPD-H 13-18 DU/AC)	0.77	0.53	MPD-H	10-14		10	4	0	18.18	26.42
TOTAL RESIDENTIAL DEVELOPMENT PARCEL AREAS	14.64	11.61				42	81	0	8.40	10.59
TOTAL ALL DEVELOPMENT PARCEL AREAS	14.64	11.61								
REMAINING VILLAGES MPD						888	2,144	380,000		

- GROSS RESIDENTIAL PARCEL DENSITY = RESIDENTIAL UNITS / GROSS DEVELOPMENT PARCEL AREA
- NET RESIDENTIAL PARCEL DENSITY = RESIDENTIAL UNITS / NET DEVELOPMENT PARCEL AREA (EXCLUDES STREETS, ROADS, AND ALLEYS, PUBLIC OR PRIVATE)

OVERALL PHASE 2 PLAT F GROSS RESIDENTIAL PARCEL DENSITY = 8.40 UNITS/ACRE (123 UNITS / 14.64)

OVERALL PHASE 2 PLAT F NET RESIDENTIAL PARCEL DENSITY = 10.61 UNITS/ACRE (123 UNITS / 11.59)

### LOT SIZE INFORMATION

#### SINGLE FAMILY LOT INFORMATION

MINIMUM PROPOSED LOT SIZE (SINGLE FAMILY ATTACHED)	1,120 SF
MAXIMUM PROPOSED LOT SIZE (SINGLE FAMILY DETACHED)	8,716 SF
AVERAGE LOT SIZE (SINGLE FAMILY ATTACHED/DETACHED)	3,808 SF (468.38 SF/123 LOTS)

### TITLE EXCEPTIONS

PARAGRAPH NUMBERS FROM FIDELITY NATIONAL TITLE INSURANCE COMPANY ORDER NO. 22001337-SC, REVISION 1 DATED SEPTEMBER 22, 2022 AT 8:00 AM.

- SUBJECT TO THE RESERVATION OF ALL COAL, OIL, GAS AND MINERAL RIGHTS, AND RIGHTS TO EXPLORE FOR THE SAME AS GRANTED BY WEYERHAEUSER COMPANY, A WASHINGTON CORPORATION, IN THE DEED RECORDED JANUARY 4, 1985 UNDER RECORDING NO. 8501040307, RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO A CONSERVATION EASEMENT EXECUTED BY PLUM CREEK LAND COMPANY AND THE CITY OF BLACK DIAMOND IN THE DEED RECORDED JANUARY 6, 2010 UNDER RECORDING NO. 20100106000723, RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO A MITIGATION AGREEMENT BETWEEN BD VILLAGE PARTNERS, LP; BD LAWSON PARTNERS, LP; AND THE CITY OF MAPLE VALLEY AS DISCLOSED BY THE DOCUMENT RECORDED APRIL 22, 2011 UNDER RECORDING NO. 20110422000249, RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO A COMPREHENSIVE SCHOOL MITIGATION AGREEMENT BETWEEN BD VILLAGE PARTNERS, LP; BD LAWSON PARTNERS, LP; THE CITY OF BLACK DIAMOND; AND ENUMCLAW SCHOOL DISTRICT AS DISCLOSED BY THE DOCUMENT RECORDED JUNE 24, 2011 UNDER RECORDING NO. 20110624001156 AND AMENDED BY THE DOCUMENT RECORDED JANUARY 30, 2015 UNDER RECORDING NO. 20150130000466, ALL RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO THE VILLAGE MASTER PLANNED DEVELOPMENT AGREEMENT EXECUTED BY THE CITY OF BLACK DIAMOND AND BD VILLAGE PARTNERS, L.P. AND RECORDED ON JANUARY 30, 2012 UNDER RECORDING NO. 20120130000655, RECORDS OF KING COUNTY, WASHINGTON. SAID AGREEMENT WAS SUBSEQUENTLY AMENDED BY THE FOLLOWING DOCUMENTS RECORDED SEPTEMBER 6, 2012, JANUARY 3, 2014, NOVEMBER 12, 2014, DECEMBER 6, 2017, OCTOBER 11, 2018, FEBRUARY 28, 2019 AND FEBRUARY 10, 2022 UNDER RECORDING NOS. 20120906000762, 20120906000763, 20140103000575, 20141112001375, 20171206000581, 20181011000281, 20190228000492, 20190228000493 AND 20220210000586, ALL RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO AN EASEMENT FOR TEMPORARY ACCESS PURPOSES AS DISCLOSED BY THE DOCUMENT RECORDED MAY 28, 2015 UNDER RECORDING NO. 20150528000482 AND SUBSEQUENTLY AMENDED BY DOCUMENTS RECORDED AUGUST 11, 2015, OCTOBER 21, 2015 AND SEPTEMBER 22, 2022 UNDER RECORDING NOS. 20150811000652, 20151021001954 AND 20220922000275, ALL RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO AN EASEMENT GRANTED TO PUGET SOUND ENERGY, INC. FOR TRANSMISSION AND DISTRIBUTION OF GAS AND ELECTRICITY PURPOSES AS DISCLOSED BY THE DOCUMENT RECORDED MAY 18, 2017 UNDER RECORDING NO. 20170518000241, RECORDS OF KING COUNTY, WASHINGTON AND SUBSEQUENTLY AMENDED BY THE DOCUMENT RECORDED MARCH 13, 2018 UNDER RECORDING NO. 20180313000130, RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO COVENANTS, CONDITIONS, RESTRICTIONS AND EASEMENTS, OMITTING THOSE WHICH ARE DISCRIMINATORY IN NATURE, AS SET FORTH IN APPLICABLE STATE OR FEDERAL LAWS, EXCEPT TO THE EXTENT THAT SAID COVENANT OR RESTRICTION IS PERMITTED BY APPLICABLE LAW, AS SET FORTH IN THE DOCUMENT RECORDED NOVEMBER 7, 2017 UNDER RECORDING NO. 20171107001312, RECORDS OF KING COUNTY, WASHINGTON. SAID DOCUMENT WAS SUBSEQUENTLY AMENDED BY DOCUMENTS RECORDED DECEMBER 27, 2017, JUNE 19, 2018, JUNE 27, 2018, AUGUST 22, 2019, DECEMBER 31, 2019 AND DECEMBER 9, 2021 UNDER RECORDING NOS. 20171227000507, 20180619000316, 20180627001396, 20190822000857, 20191231001205 AND 20211209001073, ALL RECORDS OF KING COUNTY, WASHINGTON.
- SUBJECT TO COVENANTS, CONDITIONS, RESTRICTIONS, RECITALS, RESERVATIONS, EASEMENT, EASEMENT PROVISIONS, ENCROACHMENTS, DEDICATIONS, BUILDING SETBACK LINES, NOTES, STATEMENTS, AND OTHER MATTERS, IF ANY, OMITTING THOSE WHICH ARE DISCRIMINATORY IN NATURE, AS SET FORTH IN APPLICABLE STATE OR FEDERAL LAWS, EXCEPT TO THE EXTENT THAT SAID COVENANT OR RESTRICTION IS PERMITTED BY APPLICABLE LAW, AS SET FORTH ON CITY OF BLACK DIAMOND LOT LINE ADJUSTMENT NO. PLN 21-0011 RECORDED JUNE 28, 2021 UNDER RECORDING NO. 20210628900003, RECORDS OF KING COUNTY, WASHINGTON.

### TRANSFER OF DEVELOPMENT RIGHTS

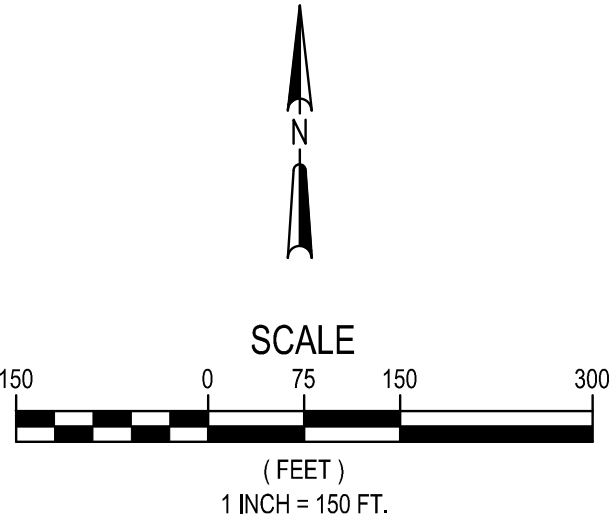
Phase	Planned Unit Count for Phase	Plat Unit Count	Previous Unit Count in Phase*	Cumulative Unit Count in Phase	% of Planned Phase Unit Count	>60% Threshold	TDR's Required	TDR's Applied
2	1320	123	687	810	61.40%	Yes	18	18

PHASE 2, PLAT A - 211 UNITS  
PHASE 2, PLAT C, DIVISIONS 1 AND 2 - 201 UNITS  
PHASE 2, PLAT D - 232 UNITS (PLAT 2D IS COMPRISED OF 233 LOTS, OF WHICH 232 LOTS ARE SINGLE FAMILY LOTS. THE REMAINING LOT IS FOR THE CLUBHOUSE  
PHASE 2, PLAT E - 43 UNITS

### LOT SUMMARY

	LOT NUMBERS	LOTS	UNITS	PRODUCT TYPE	GARAGE
DIVISION 1					
	1-20	20	20	SF DETACHED	FRONT
	61-77	17	17	SF DETACHED	FRONT
		37	37		
DIVISION 2					
	21-35	15	15	SF DETACHED	FRONT
	36-45	10	10	SF DETACHED	ALLEY
	46-60	15	15	SF DETACHED	FRONT
		40	40		
DIVISION 3					
	NONE	0	0	NOT APPLICABLE	NOT APPLICABLE
		0	0		
DIVISION 4					
	78-101	24	24	MF ATTACHED	ALLEY
	102-119	18	18	MF ATTACHED	FRONT
	120-123	4	4	SF ATTACHED	FRONT
		46	46		
DIVISION 5					
	NONE	0	0	NOT APPLICABLE	NOT APPLICABLE
		0	0		
DIVISION 6					
	NONE	0	0	NOT APPLICABLE	NOT APPLICABLE
		0	0		
DIVISION 7					
	NONE	0	0	NOT APPLICABLE	NOT APPLICABLE
		0	0		
TOTAL		123	123		

REFER TO SHEET CV3 FOR DIVISION LINES

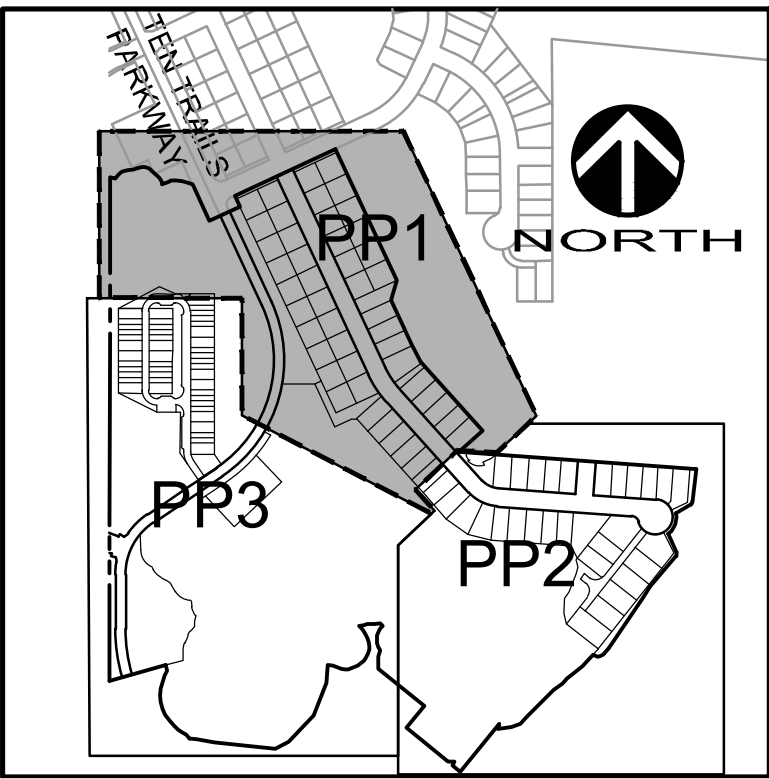


### PRELIMINARY





PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.



PRELIMINARY PLAT  
PRELIMINARY PLAT PLAN

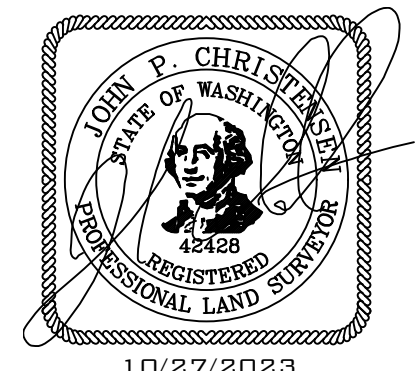
TEN TRAILS  
PHASE 2 - PLAT F

WASHINGTON

CITY OF BLACK DIAMOND

REVIEWED BY:	DATE:	BY:	CHK:
DATE:	10/27/23	DATE:	10/27/23
NO.:	1	NO.:	1
REVISIONS:		REVISIONS:	
1	10/27/23	1	10/27/23
2	10/27/23	2	10/27/23
3	10/27/23	3	10/27/23

PRELIMINARY



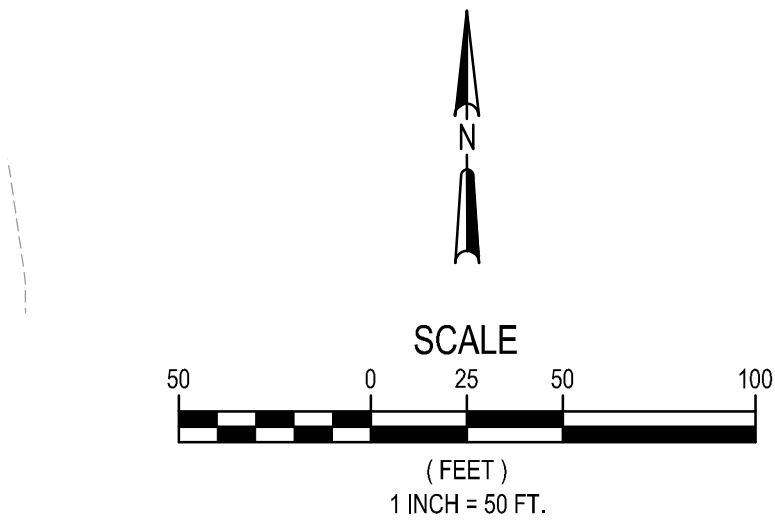
CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.

PP1



TRACT DESIGNATION LEGEND

- AC - ACCESS
- LA - LANDSCAPE
- NL - NATURAL LANDSCAPE
- PA - PEDESTRIAN ACCESS
- PK - PARK
- SA - SENSITIVE AREA AND BUFFER
- UT - UTILITY
- FD - FUTURE DEVELOPMENT

NOTES:  
ALL ROAD SECTIONS TO BE PER THE VILLAGES MPD DEVELOPMENT AGREEMENT.  
THE UTILITY PIPES AND CATCH BASINS SHOWN IN THIS PRELIMINARY PLAT ARE SCHEMATIC. THE UTILITY LINE TYPES AND LOCATIONS SHOULD ALSO BE CONSIDERED SCHEMATIC AND ARE REPRESENTATIVE OF THE GENERAL PRESENCE OF THE UTILITY ONLY. THE LOCATION AND CONFIGURATION OF UTILITY PIPES IS NOT APPROVED IN THIS APPLICATION.  
BASED ON GEOTECHNICAL INVESTIGATION, GROUNDWATER FROM THIS SITE WILL FLOW AWAY FROM ROCK CREEK AND LAKE SAWYER.

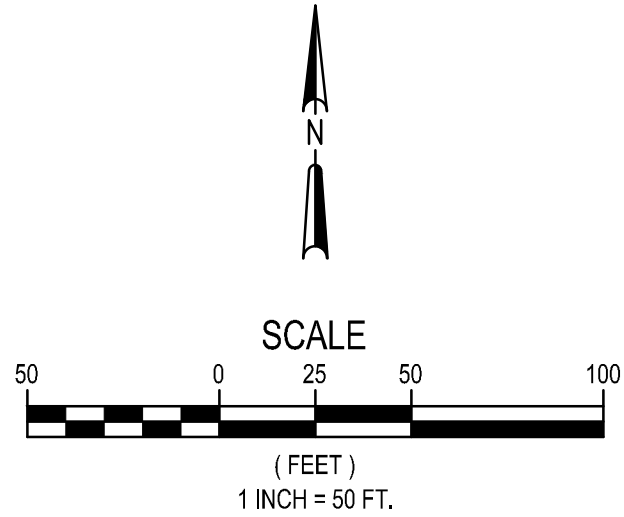
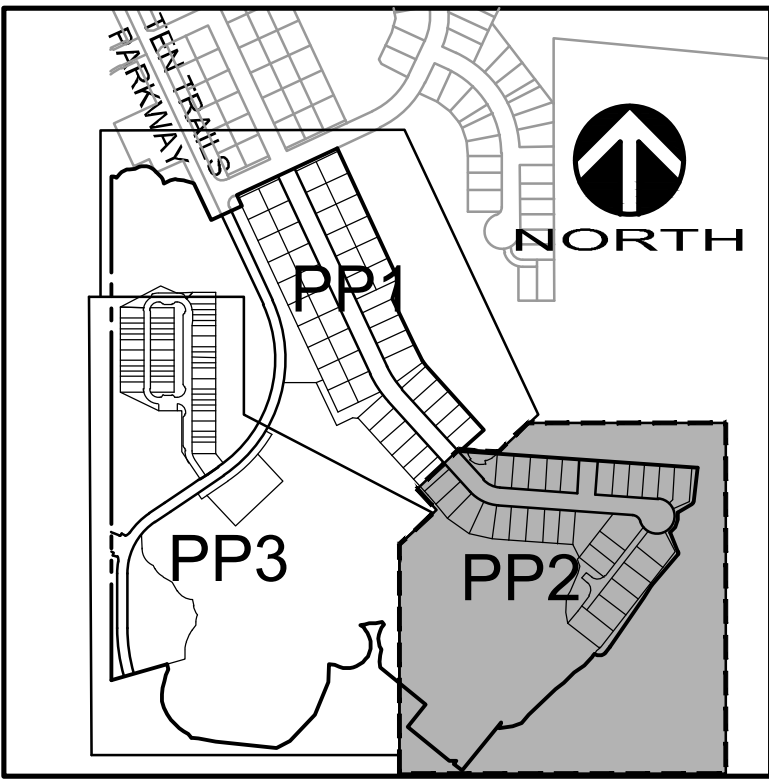
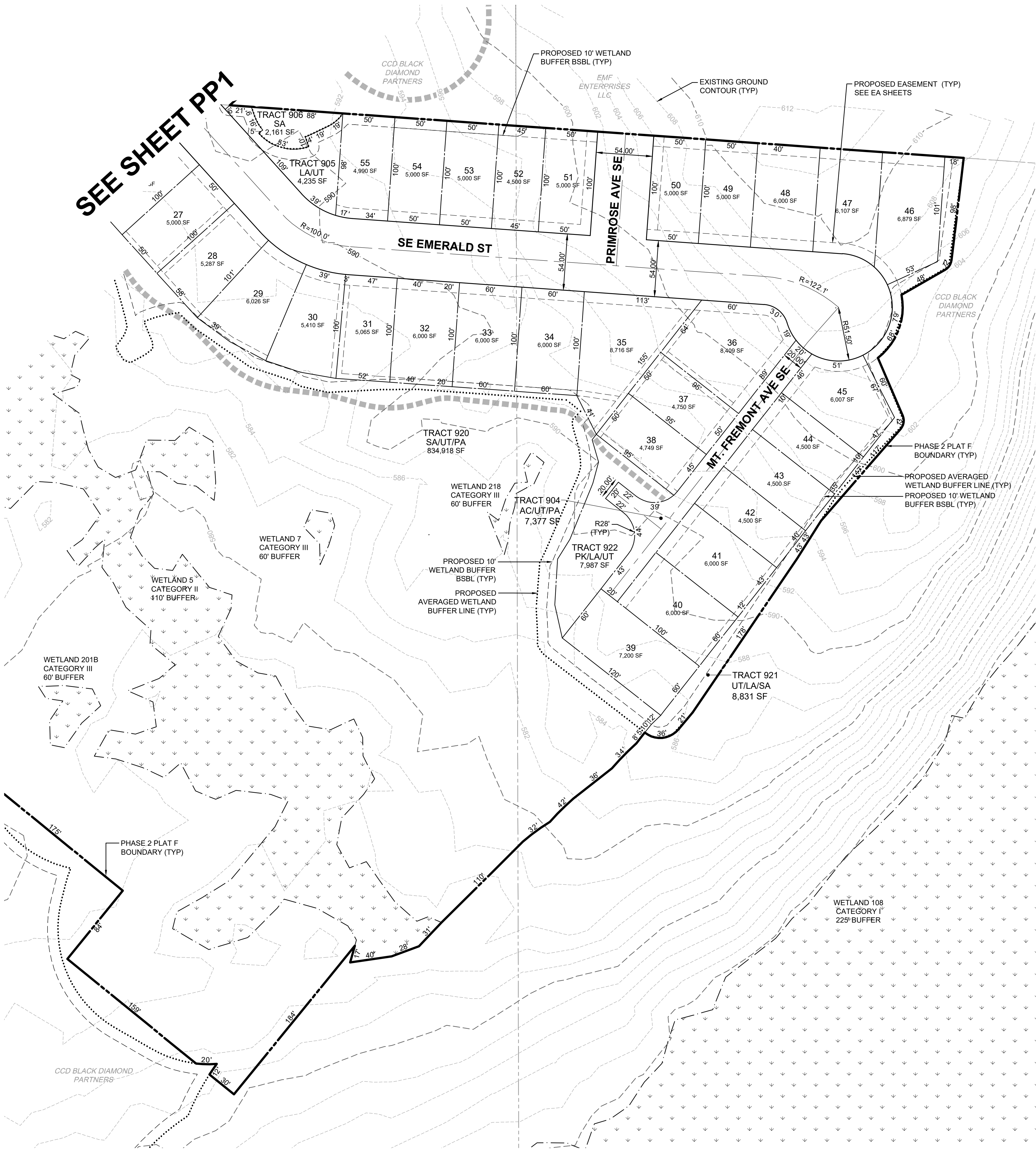
SEE SHEET PP3

SEE SHEET PP3

SEE SHEET PP2



PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.



TRACT DESIGNATION LEGEND

- AC - ACCESS
- LA - LANDSCAPE
- NL - NATURAL LANDSCAPE
- PA - PEDESTRIAN ACCESS
- PK - PARK
- SA - SENSITIVE AREA AND BUFFER
- UT - UTILITY
- FD - FUTURE DEVELOPMENT

NOTES:  
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THE UTILITY PIPES AND CATCH BASINS SHOWN IN THIS PRELIMINARY PLAT ARE SCHEMATIC. THE UTILITY LINE TYPES AND LOCATIONS SHOULD ALSO BE CONSIDERED SCHEMATIC AND ARE REPRESENTATIVE OF THE GENERAL PRESENCE OF THE UTILITY ONLY. THE LOCATION AND CONFIGURATION OF UTILITY PIPES IS NOT APPROVED IN THIS APPLICATION.  
BASED ON GEOTECHNICAL INVESTIGATION, GROUNDWATER FROM THIS SITE WILL FLOW AWAY FROM ROCK CREEK AND LAKE SAWYER.



DAVID EVANS AND ASSOCIATES INC.  
20300 Woodinville Snohomish Rd NE  
Woodinville Washington 98072  
Phone: 425.415.2000

PRELIMINARY PLAT  
PRELIMINARY PLAT PLAN

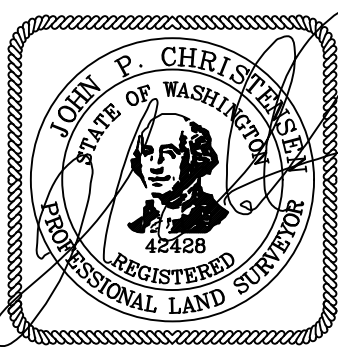
TEN TRAILS  
PHASE 2 - PLAT F

WASHINGTON

CITY OF BLACK DIAMOND

REVIEWED BY:	DATE:	BY:
DATE: 10/27/2023	10/27/2023	OK
NO. 1	10/27/2023	OK
NO. 2	10/27/2023	OK
NO. 3	10/27/2023	OK

PRELIMINARY



CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP  
FIRST SUBMITTAL DATE: 3/8/23

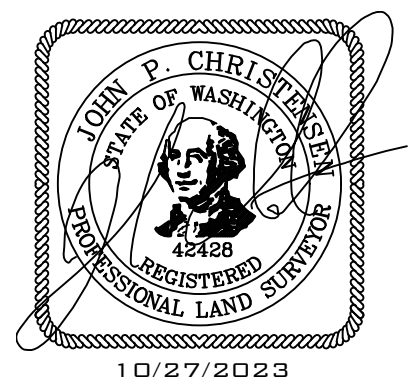
PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
PP2



REVIEWED BY:	DATE:	BY:
DATE: 10/27/2023	REVISION:	OK
NO. 1	DATE: 10/27/2023	BY: JAP
2	DATE: 10/27/2023	BY: JAP
3	DATE: 10/27/2023	BY: JAP

PRELIMINARY



CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23  
PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
PP3

SEE SHEET PP1

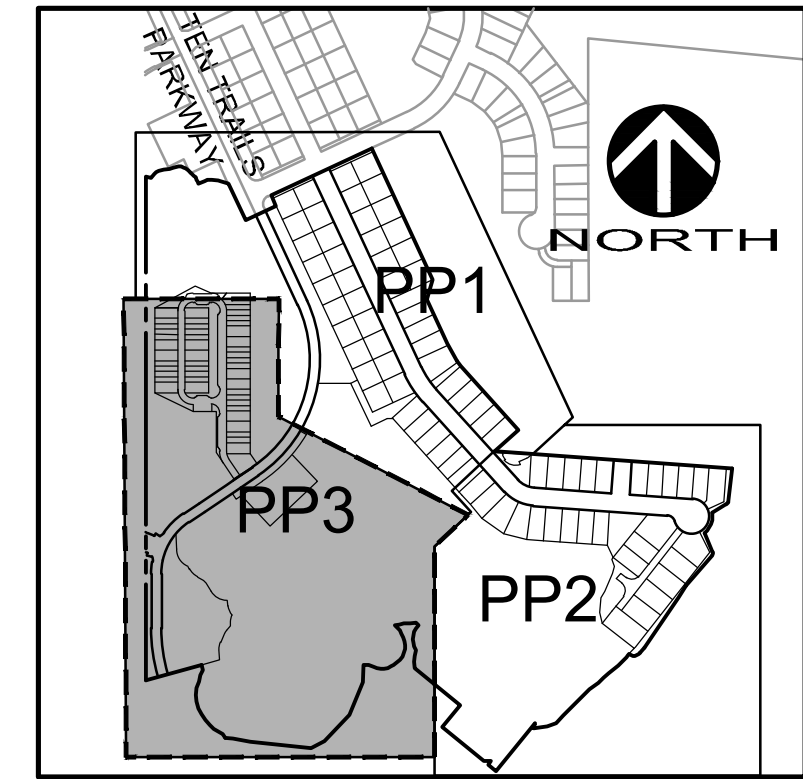
PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.

SEE LEFT

SEE SHEET PP1

SEE SHEET PP2

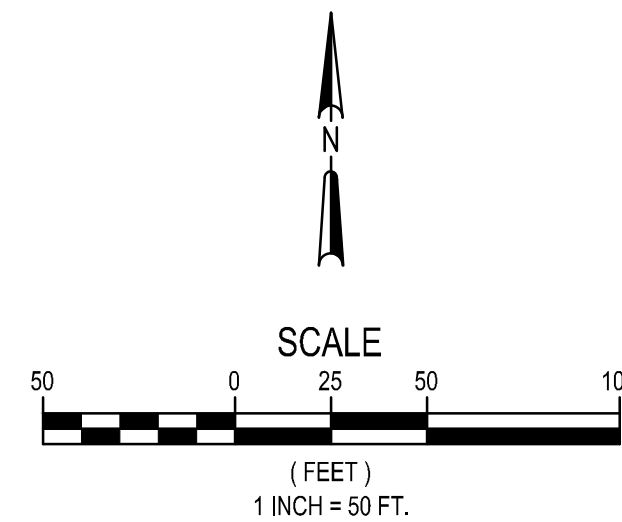
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TRACT DESIGNATION LEGEND

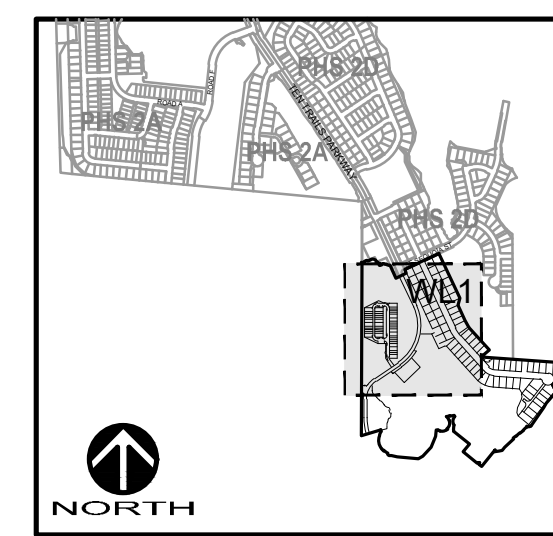
AC - ACCESS  
LA - LANDSCAPE  
NL - NATURAL LANDSCAPE  
PA - PEDESTRIAN ACCESS  
PK - PARK  
SA - SENSITIVE AREA AND BUFFER  
UT - UTILITY  
FD - FUTURE DEVELOPMENT

NOTES:  
ALL ROAD SECTIONS TO BE PER THE VILLAGES MPD DEVELOPMENT AGREEMENT.  
THE UTILITY PIPES AND CATCH BASINS SHOWN IN THIS PRELIMINARY PLAT ARE SCHEMATIC. THE UTILITY LINE TYPES AND LOCATIONS SHOULD ALSO BE CONSIDERED SCHEMATIC AND ARE REPRESENTATIVE OF THE GENERAL PRESENCE OF THE UTILITY ONLY. THE LOCATION AND CONFIGURATION OF UTILITY PIPES IS NOT APPROVED IN THIS APPLICATION.  
BASED ON GEOTECHNICAL INVESTIGATION, GROUNDWATER FROM THIS SITE WILL FLOW AWAY FROM ROCK CREEK AND LAKE SAWYER.



Plot Date: 10/27/2023 12:37 PM  
Save Date: 10/27/2023 12:42 PM  
By: Kevin Puckett  
File: P:\OAKPCBDP6001\OAKPCBDP6001\_Ten\_Trails\_Plat\_2F\_Sheets\Preliminary\_Plat\_PP3\_P\_OAKPCBDP6001\_P2F.dwg





KEY MAP  
NOT TO SCALE

COMPLEX 1			
BUFFER AVERAGING AREA	BUFFER ADDITION (SF)	BUFFER REDUCTION (SF)	NET BUFFER GAIN (SF)
2/212/214/105 (AREA 1-1)		2,668	
2/212/214/105 (AREA 1-2)		1,330	
2/212/214/105 (AREA 1-3)	119		
2/212/214/105 (AREA 1-4)		3,525	
2/212/214/105 (AREA 1-5)	6,809		
2/212/214/105 (AREA 1-6)		1,010	
2/212/214/105 (AREA 1-7)	5,282		
2/212/214/105 (AREA 1-8)		3,448	
TOTALS	12,210	11,981	229

WETLAND BUFFER ADDITION

WETLAND BUFFER REDUCTION

TEMPORARY WETLAND BUFFER REDUCTION

## BUFFER CREATION

EXISTING WETLAND BUFFER LINE

..... PROPOSED WETLAND BUFFER LINE

AC - ACCESS  
LA - LANDSCAPE  
NL - NATURAL LANDSCAPE  
PA - PEDESTRIAN ACCESS  
PK - PARK  
SA - SENSITIVE AREA AND BUFFER  
UD - UTILITY  
FD - FUTURE DEVELOPMENT

**PRELIMINARY PLAT  
WETLAND BUFFER AVERAGING**

## TEN TRAILS PHASE 2 - PLATF

CITY OF BLACK DIAMOND

REVIEWED BY:		DATE:
DATE	REVISION	BY CK
8/21/23	CLIENT REVISIONS	DAMTPM
9/6/23	OWNER/CLIENT REVISIONS	DAMTPM
10/27/23	PER CITY REVIEW COMMENTS DATED 10/18/23	KMTPM

10/27/23

CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
**OAKPCBDP6001**

SHEET NO.

# WL1

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 By: Kmpu File: P:\OAKP\CBDP6001\0400CAD\EC\PREL\MINARYTen\_Trails\Plat 2F\Sheets\Preliminary\PlatWL\_P-OAKP\CBDP6001-P2F.dwg

SEE SHEET WL3

SEE SHEET WL2

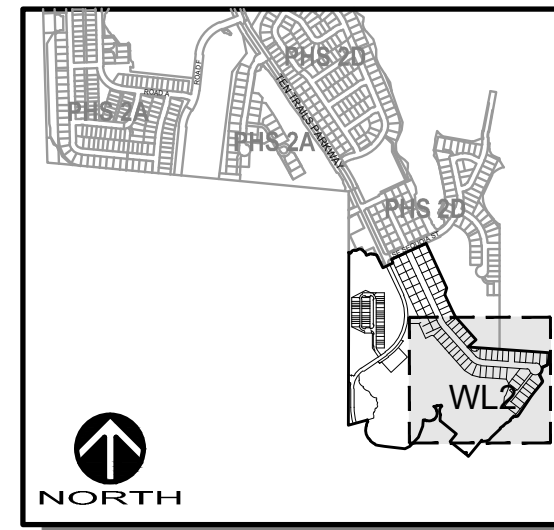
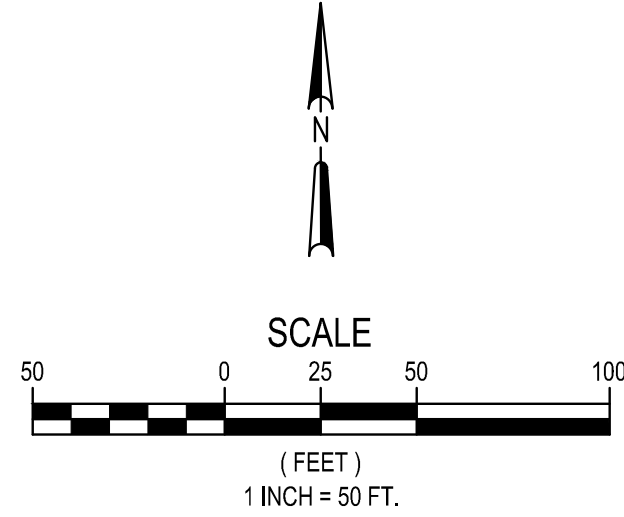


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SEE SHEET WL3

SEE SHEET WL1

PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.



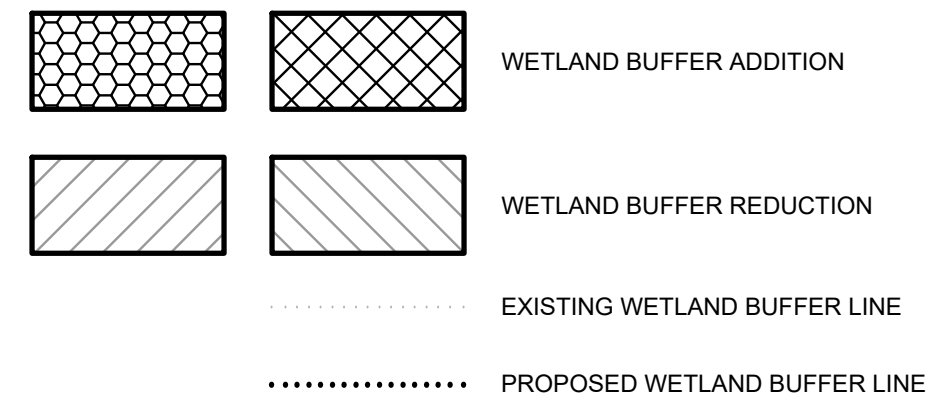
KEY MAP  
NOT TO SCALE

COMPLEX 2			
BUFFER AVERAGING AREA	BUFFER ADDITION (SF)	BUFFER REDUCTION (SF)	NET BUFFER GAIN (SF)
218/7/5/108 (AREA 2-1)	1,545		
218/7/5/108 (AREA 2-2)		8,974	
218/7/5/108 (AREA 2-3)		3,105	
218/7/5/108 (AREA 2-4)	34,379		
218/7/5/108 (AREA 2-5)		4,100	
218/7/5/108 (AREA 2-6)		2,165	
218/7/5/108 (AREA 2-7)		7,092	
TOTALS	35,924	25,436	10,488

WETLAND 215			
BUFFER AVERAGING AREA	BUFFER ADDITION (SF)	BUFFER REDUCTION (SF)	NET BUFFER GAIN (SF)
215 (AREA 215-1)		230	
215 (AREA 215-2)	247		
TOTALS	247	230	17

WETLAND 218			
BUFFER AVERAGING AREA	BUFFER ADDITION (SF)	BUFFER REDUCTION (SF)	NET BUFFER GAIN (SF)
218 (AREA 218-1)	5,209		
218 (AREA 218-2)		4,170	
218 (AREA 218-3)		318	
TOTALS	5,209	4,488	721

WETLAND BUFFER LEGEND

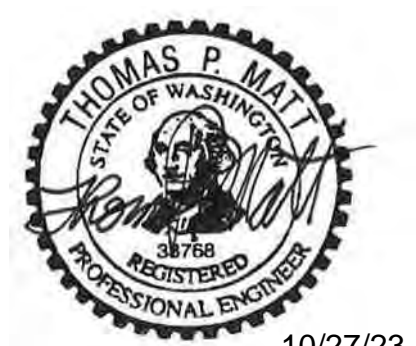


TRACT DESIGNATION LEGEND

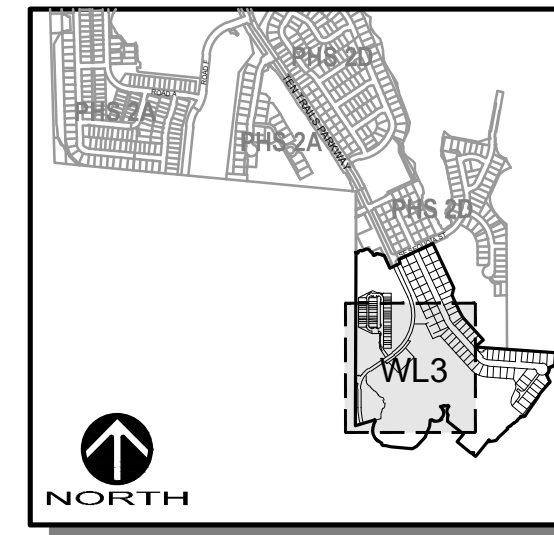
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REVIEWED BY:	DATE:	BY:
NO. 1	DATE	BY
1	10/27/23	DAVID
2	10/27/23	DAVID
3	10/27/23	DAVID

PRELIMINARY





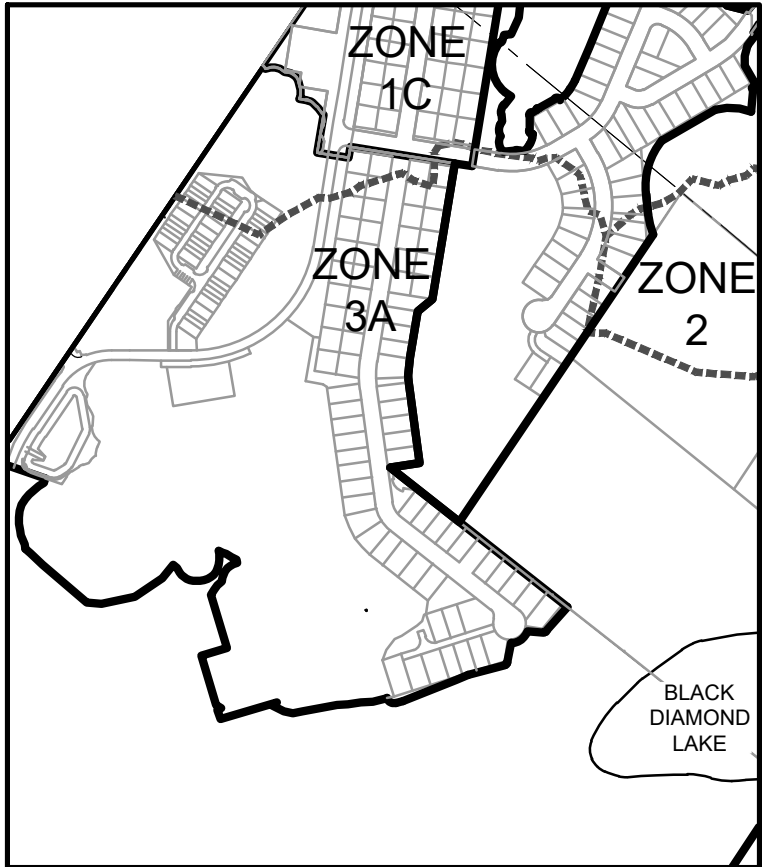
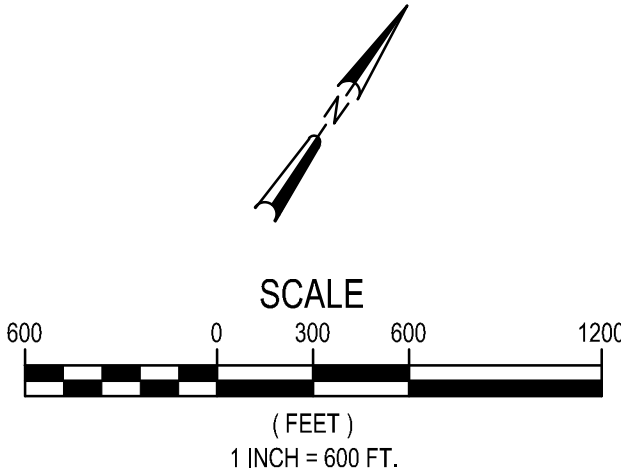


COMPLEX 4			
BUFFER AVERAGING AREA	BUFFER ADDITION (SF)	BUFFER REDUCTION (SF)	NET BUFFER GAIN (SF)
200/201/102/3 (AREA 4-1)	14,821		
200/201/102/3 (AREA 4-2)		12,764	
200/201/102/3 (AREA 4-3)	4,465		
200/201/102/3 (AREA 4-4)	1,048		
200/201/102/3 (AREA 4-5)		1,411	
TOTALS	20,334	14,175	6,159

SEE LEFT

SEE RIGHT

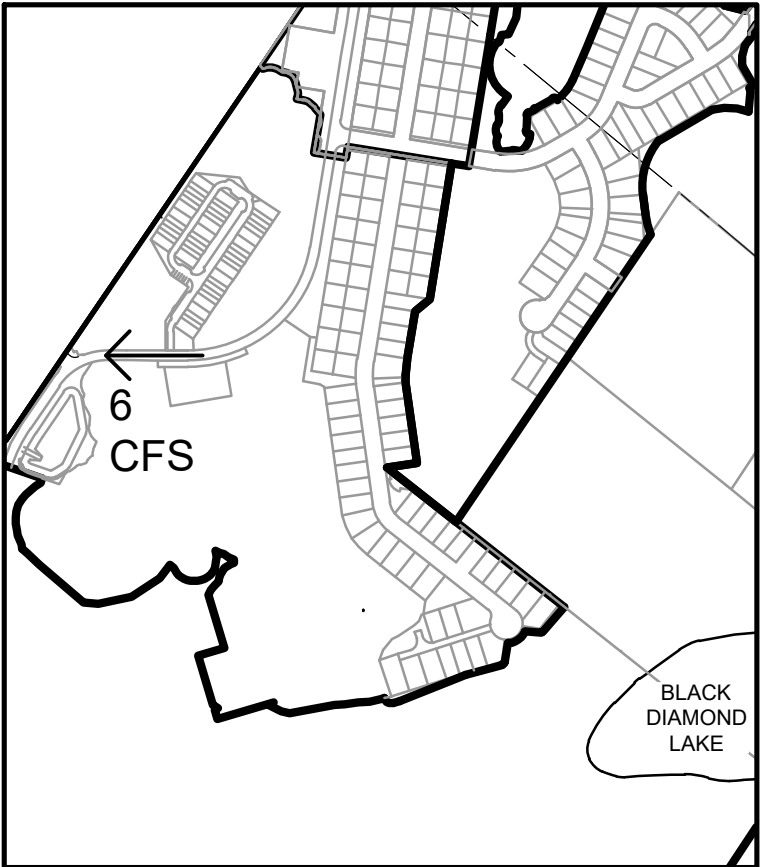




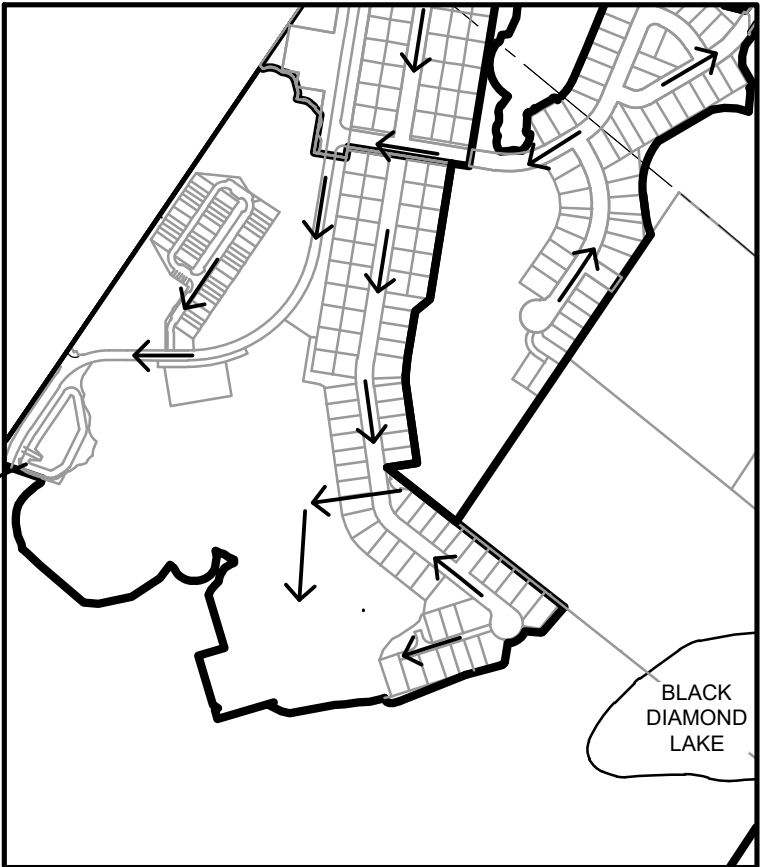
STORMWATER MANAGEMENT ZONES

1C NOT TRIBUTARY TO HORSESHOE LAKE OR ROCK CREEK LAKE  
SAWYER RUNOFF NOT REQUIRED FOR WETLAND HYDROLOGY TO  
BE CONVEYED TO OFFSITE STORMWATER FACILITY.

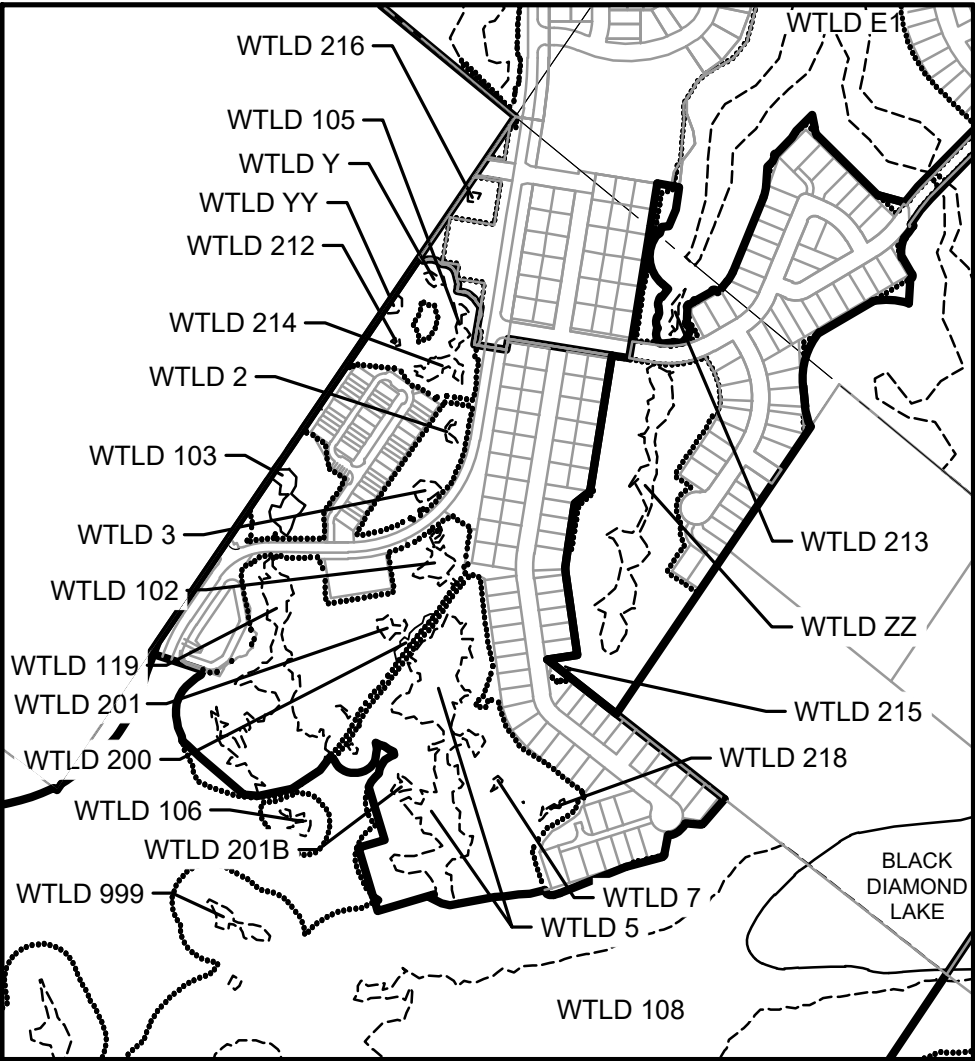
3A TRIBUTARY TO BLACK DIAMOND LAKE. RUNOFF FROM ROOFTOPS  
MAY BE USED TO PROVIDE WETLAND HYDROLOGY. RUNOFF NOT  
REQUIRED FOR WETLAND HYDROLOGY TO BE CONVEYED TO  
OFFSITE STORMWATER FACILITY.



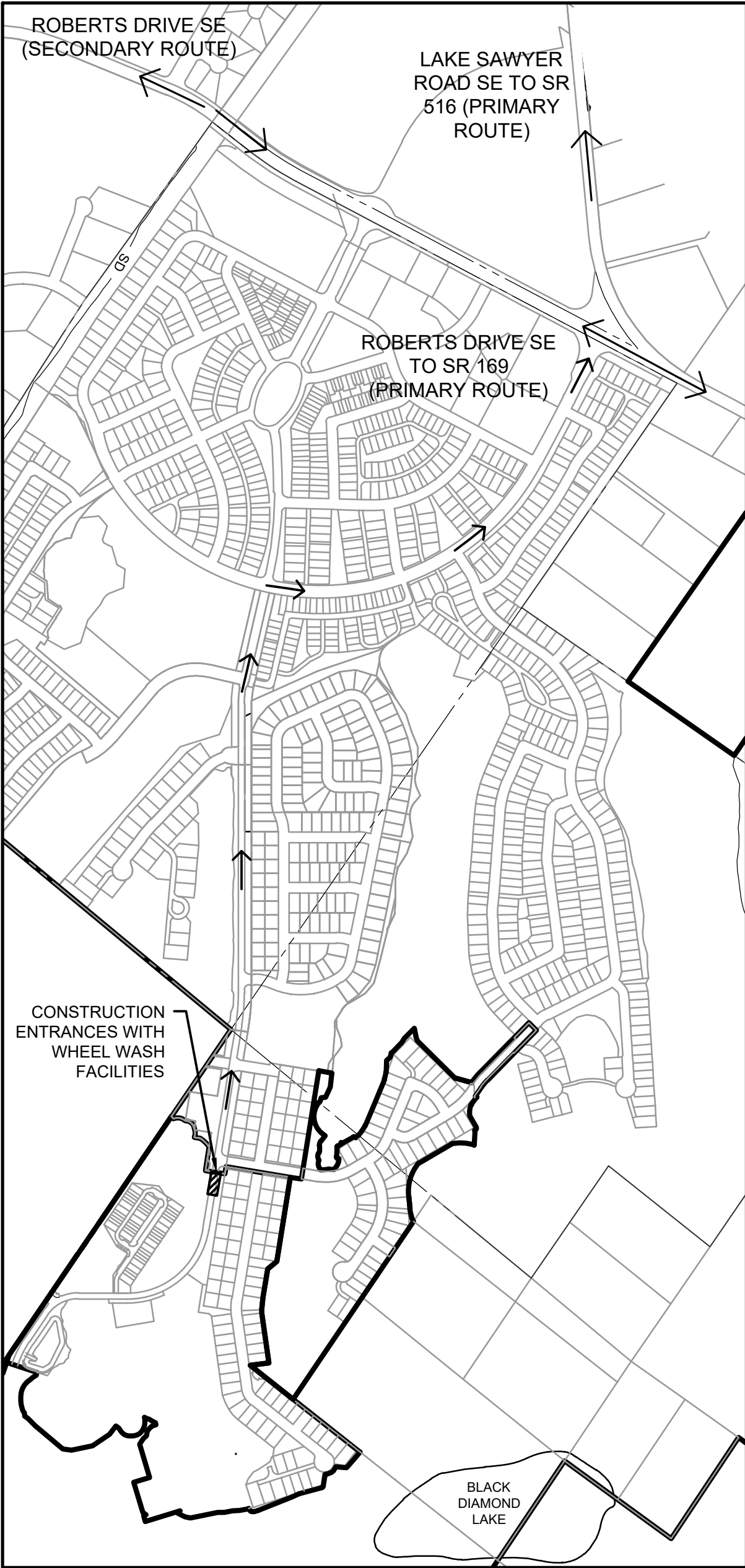
CAPACITY REQUIREMENTS



EMERGENCY OVERFLOW ROUTES



CRITICAL AREAS

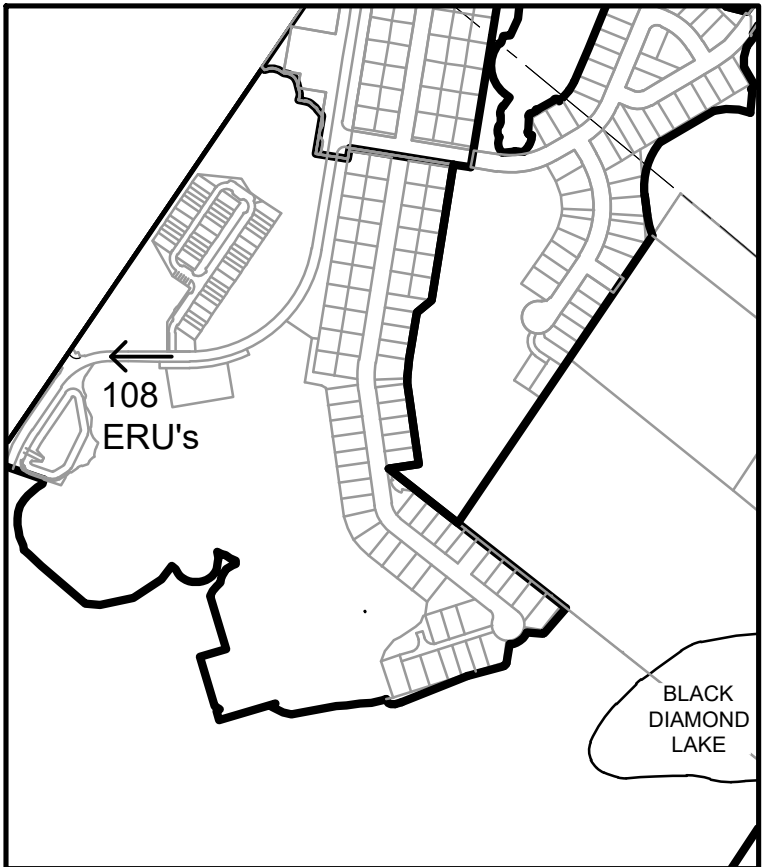


- NOTES:
- MASTER DEVELOPER SHALL INCLUDE THE FOLLOWING PROVISION IN CLEARING, GRADING AND CONSTRUCTION CONTRACTS: "EXCEPT FOR THE EASTERLY 1,000 FEET OF SE GREEN VALLEY ROAD, SE GREEN VALLEY ROAD SHALL NOT BE USED AS A CONSTRUCTION HAUL ROUTE BY CONTRACTORS OR ITS AGENTS."
  - CONSTRUCTION TRAFFIC CONTROL DESIGN WILL BE PROVIDED AS PART OF FINAL ENGINEERING PLANS FOR REVIEW AND APPROVAL BY THE CITY OF BLACK DIAMOND.

## PRELIMINARY HAUL ROUTE PLAN

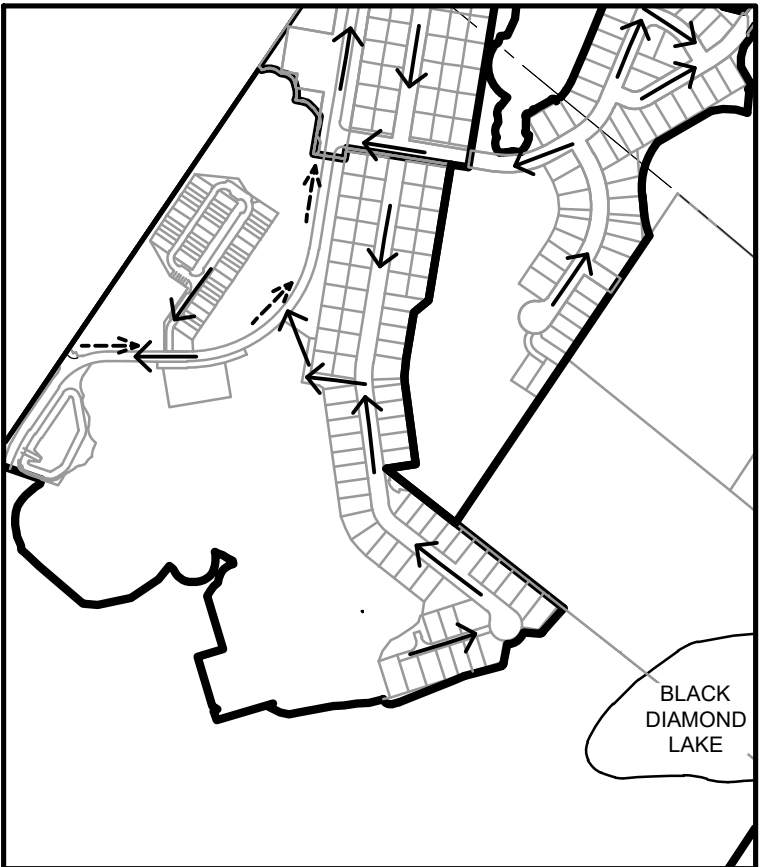
## UTILITY ERU CALCULATION

ASSUMPTIONS:  
1 SFR = 1 ERU  
1 MF = 0.67 ERU  
  
77 SFR (1 ERU) + 46 MF (0.67 ERU) = 108 ERU

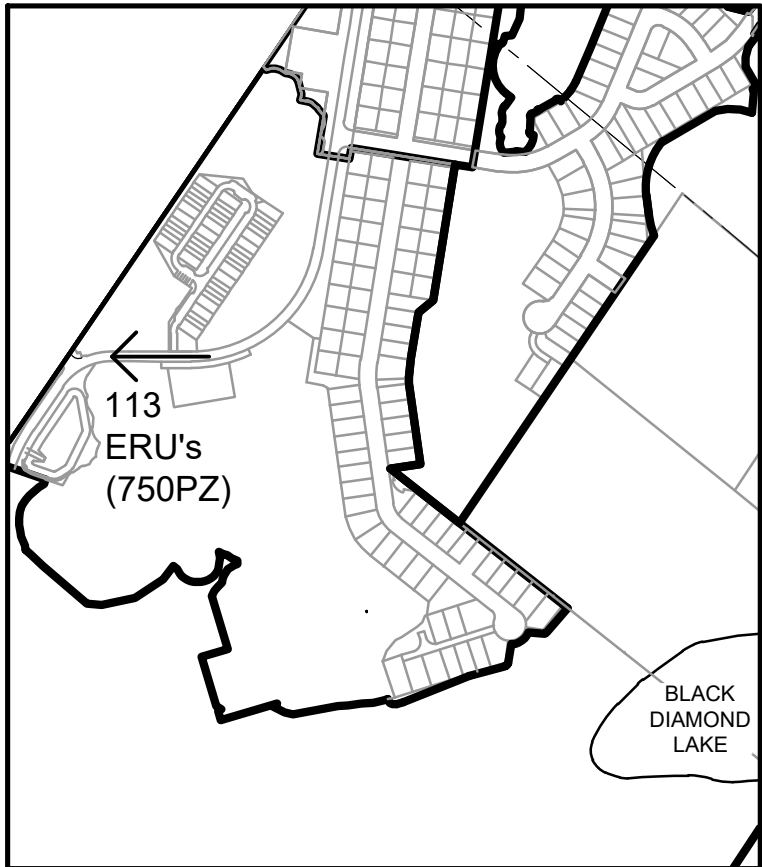


SEWER CAPACITY REQUIREMENTS

WASTEWATER FLOW THIS PROJECT:  
AVERAGE WET DAY FLOW: 175 GPD/ERU x 108 ERU = 18,900 GPD  
INFILTRATION/INFLOW INCLUDED IN THE 175 GPD/ERU VALUE  
TOTAL PEAK FLOW FOR PROJECT: 18,900 GPD  
(2.5)(18,900 GPD)/24 HRS = 1,969 GPH (0.07 CFS)

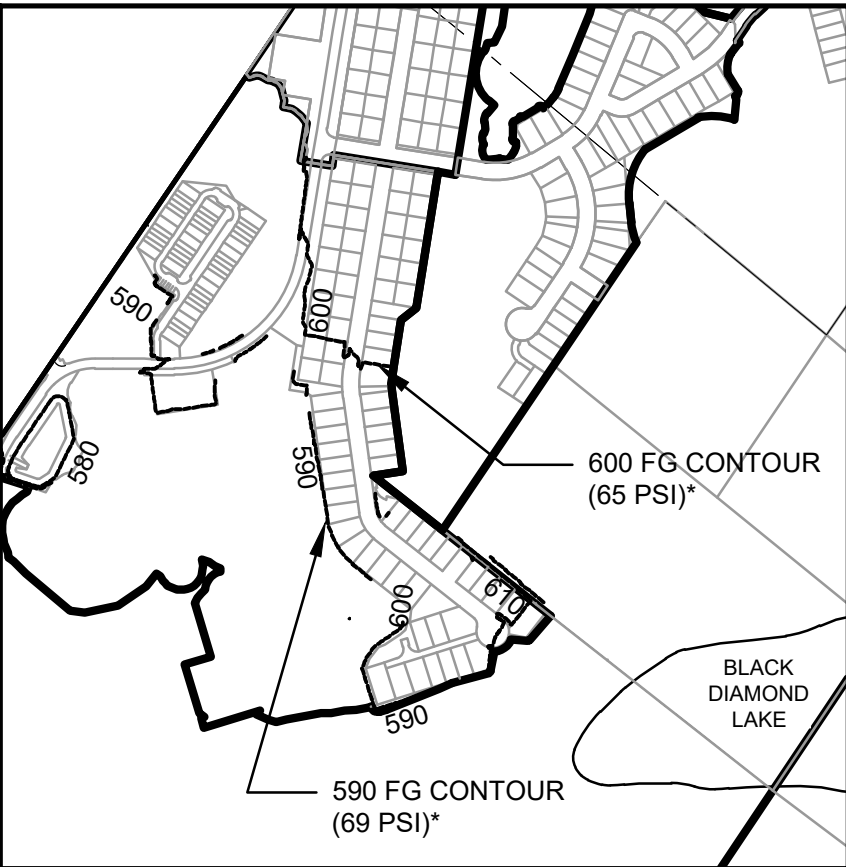


SEWER CONVEYANCE PIPE ROUTES



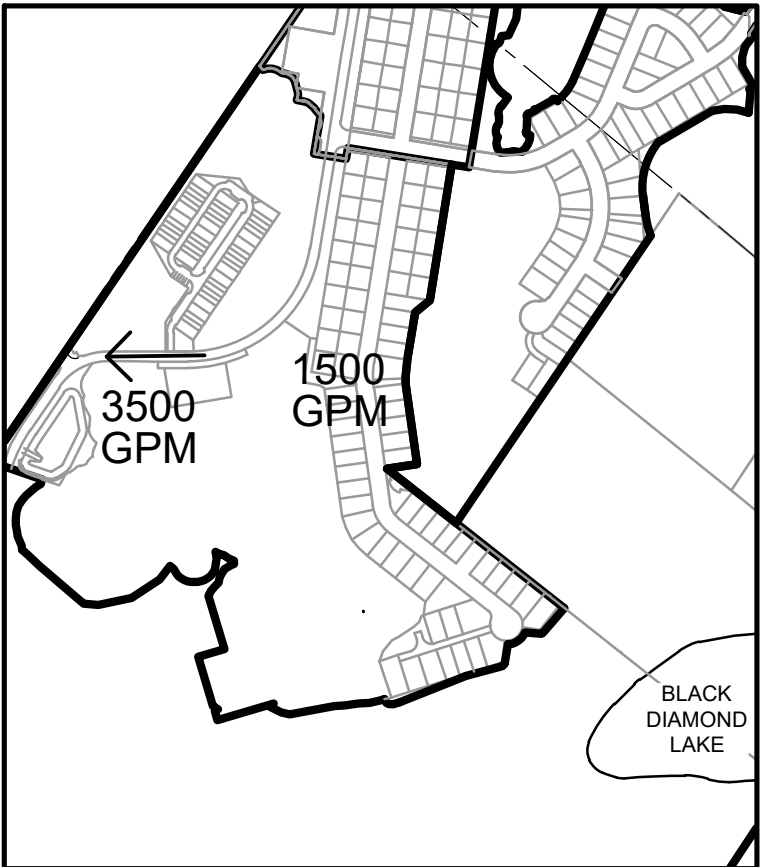
WATER CAPACITY REQUIREMENTS

WATER DEMAND THIS PROJECT:  
AVERAGE DAILY DEMAND (ADD) = 187 GPD/ERU x 108 ERU = 20,196 GPD  
MAXIMUM DAILY DEMAND (MDD) = 2.5(ADD) = 50,490 GPD



PRESSURE REQUIREMENTS

( ) APPROXIMATE STATIC WATER  
PRESSURE BASED ON 750 PRESSURE ZONE



FIREFLOW REQUIREMENTS

2500 GPM IS THE FIRE FLOW REQUIREMENT FOR MASTER PLANNED DEVELOPMENT OVERLAY PER BODDGS 6-1.05.4. THIS FIRE FLOW REQUIREMENT BY ZONING IS TO COVER ALLOWED USES WITHIN THE ZONING. BASED ON DISCUSSIONS WITH MDRT STAFF THE FIRE FLOW REQUIREMENT WILL BE REDUCED TO 1500 GPM FOR SINGLE FAMILY RESIDENTIAL DEVELOPMENT WHEN PROPOSED LAND USE IS CONFIRMED. FIRE FLOW REQUIREMENT FOR MULTI-FAMILY AREA TO BE CONFIRMED BY THE FIRE MARSHAL.

## WATER AVAILABILITY

WATER AVAILABILITY IS PROVIDED IN SECTION 7.2.1 OF THE VILLAGES MASTER PLANNED DEVELOPMENT DEVELOPMENT AGREEMENT (DEVELOPMENT AGREEMENT). THE DEVELOPMENT AGREEMENT NOTES THAT ANY IMPLEMENTING PROJECT APPLICATION PROCESS THAT CALLS FOR A CERTIFICATE OF WATER AVAILABILITY SHALL BE SATISFIED BY REFERENCE TO THE AGREEMENT.

## SEWER AVAILABILITY

SEWER AVAILABILITY IS PROVIDED IN SECTION 7.3.1 OF THE VILLAGES MASTER PLANNED DEVELOPMENT DEVELOPMENT AGREEMENT (DEVELOPMENT AGREEMENT). THE DEVELOPMENT AGREEMENT NOTES THAT ANY IMPLEMENTING PROJECT APPLICATION PROCESS THAT CALLS FOR A CERTIFICATE OF SEWER AVAILABILITY SHALL BE SATISFIED BY REFERENCE TO THE AGREEMENT.

Ten Trails Tributary Area ERUs Tabulation				
	Residential Units (RU)	Multi-family Residential Units (MFRU)	Comm./Retail (Sq. Ft)	
	SF and attached up to 3 Units	Attached 4 or more units & MF	1 ERU per 1200 Sq Ft	ERU
	1 ERU per RU	0.67 ERU per MFRU		
Description				
P1B Plat A	133	125	205,000	387.58
P1B Plat A FDT				0.00
P1B Res Add				0.00
P1A IT1	65			65.00
P1A IT2	72	45		102.15
P1A Comm			190,000	158.33
P1A Comm Res				0.00
P1A IT3	169			169.00
P1A V10		176		117.92
P1A V13	62			62.00
P1A V14	2	48		34.16
P1A V24	61			61.00
PP1A School			34,284	28.57
P2PC Div. 1	111			111.00
P2PC Div. 2	90			90.00
P2PA	211			211.00
P2PD	232			232.00
P2PD CH	8			8.00
Plat 2E	43			43.00
Offsite				0.00
Plat 2F	77	46		107.82
Total Trib w/o 2F				0.00
Total Trib	1336	440	429,284	1,988.54

REVIEWED BY:	DATE:	BY:
DATE	10/27/23	OK
REVISION	10/27/23	OK
CLIENT REVISIONS	10/27/23	OK
PER CITY REVIEW COMMENTS DATED 10/18/23	10/27/23	OK

## PRELIMINARY



10/27/23

CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

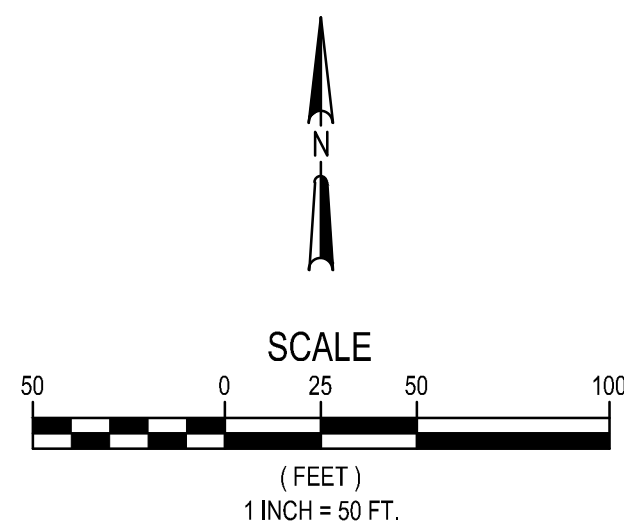
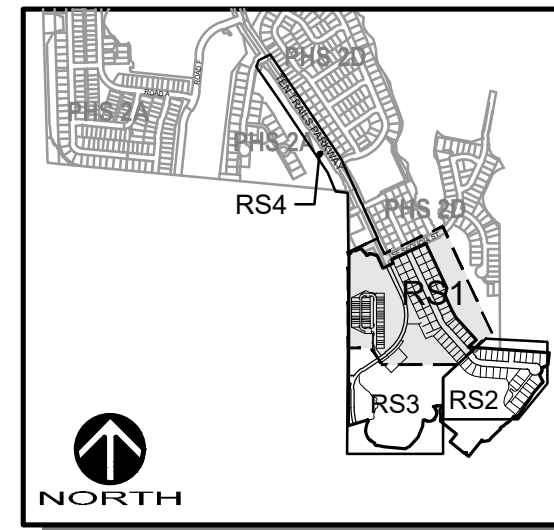
FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.

UA1





LEGEND	
ROAD, UTILITIES AND SITE	
	BOUNDARY
	RIGHT-OF-WAY
	LOT LINE
	GUTTER, FLOW, & CURB LINE
	CENTER LINE
	EASEMENT
	BUILDING SETBACK LINE (BSBL)
	EDGE OF WETLAND
	BUFFER LINE
	GUARDRAIL
	10' PROPOSED CONTOURS
	2' PROPOSED CONTOURS
	RETAINING WALL
	ROCKERY
	DITCH FLOWLINE
	SD STORM DRAIN PIPE
	FM STORM FORCE MAIN
	SS SEWER MAIN
	W WATER MAIN
	SOFT SURFACE TRAIL PATH
	CURB RAMP
	CURB RAMP (FLARED)
	CATCH BASIN, TYPE I
	CATCH BASIN, TYPE II
	OVERFLOW STRUCTURE
	PIPE FLOW
	INTAKE OR OUTFALL
	SEWER MANHOLE
	PIPE FLOW
	FIRE HYDRANT
	PROPOSED GRAVEL
	PROPOSED ASPHALT PAVEMENT
	PROPOSED SIDEWALK
	PROPOSED CONCRETE
	PROPOSED GRASSCRETE

STORMWATER FORCE MAIN LAYOUT IS SCHEMATIC AND ACTUAL ALIGNMENTS WILL BE DEVELOPED IN CONJUNCTION WITH THE UTILITY PERMIT.

WHEN A ROADWAY IS ADJACENT TO A WETLAND, GEOTECHNICAL ENGINEER REVIEW SHALL BE REQUIRED DURING THE UTILITY CONSTRUCTION TO ENSURE THAT ANY ADJACENT PROXIMATE WETLAND DOES NOT DEWATER THROUGH THE FREE-DRAINING UTILITY TRENCH BACKFILL.

TRACT DESIGNATION LEGEND

- AC - ACCESS
- LA - LANDSCAPE
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SEE SHEET "RD1" FOR PROPOSED ROAD SECTIONS

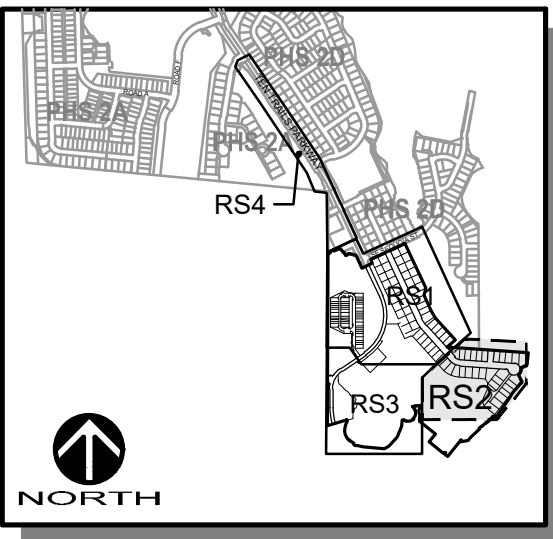
SEE SHEET RS2

SEE SHEET RS3

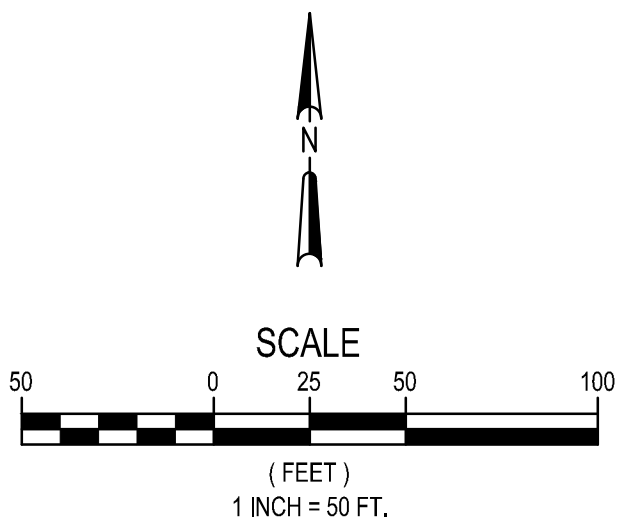
SEE SHEET RS4



PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.

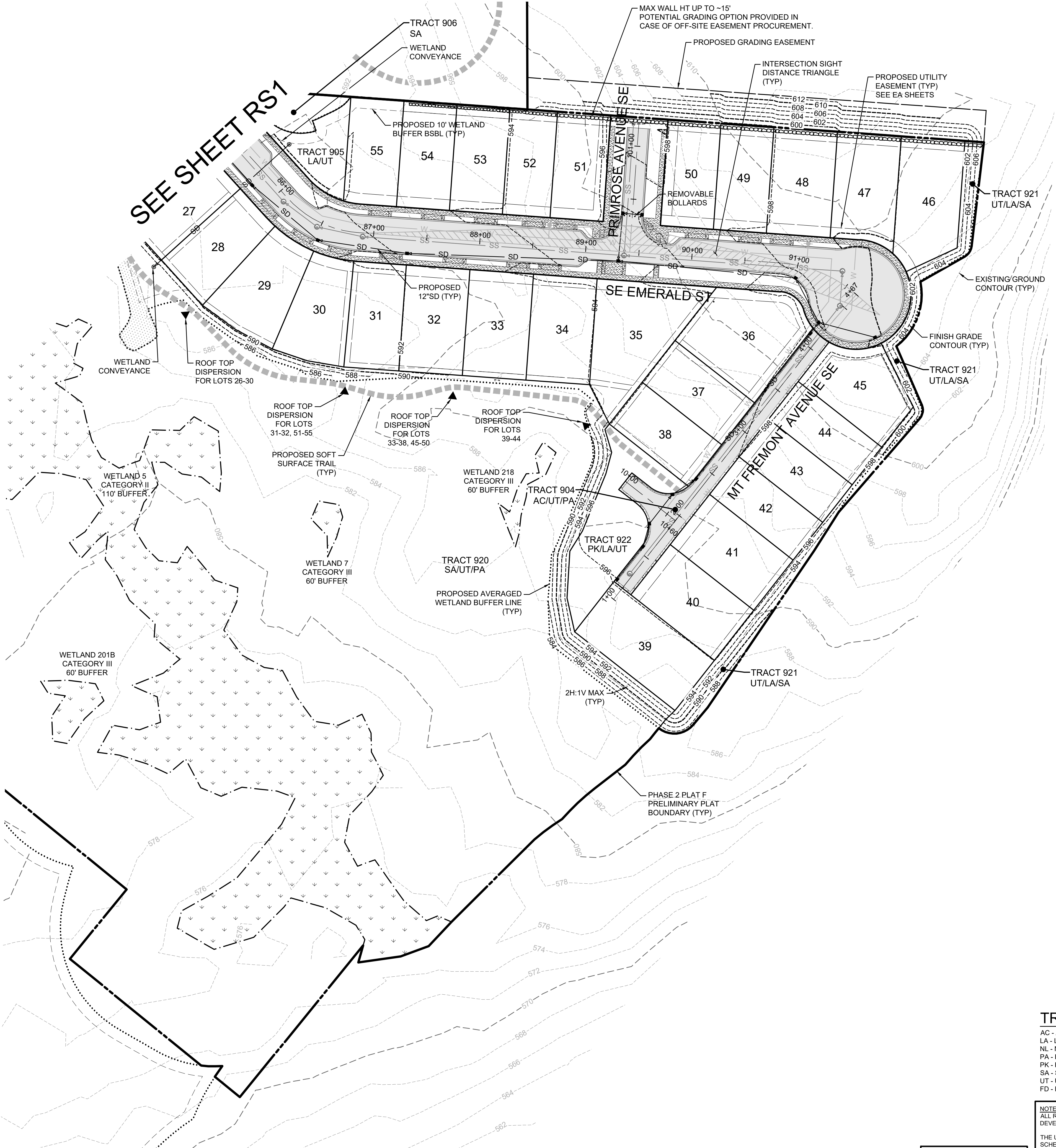


KEY MAP  
NOT TO SCALE



SEE SHEET RS3

SEE SHEET RS1



LEGEND			
ROAD, UTILITIES AND SITE			
	BOUNDARY		CURB RAMP
	RIGHT-OF-WAY		CURB RAMP (FLARED)
	LOT LINE		CATCH BASIN, TYPE I
	GUTTER, FLOW, & CURB LINE		CATCH BASIN, TYPE II
	CENTER LINE		OVERFLOW STRUCTURE
	EASEMENT		PIPE FLOW
	BUILDING SETBACK LINE (BSBL)		INTAKE OR OUTFALL
	EDGE OF WETLAND		SEWER MANHOLE
	BUFFER LINE		PIPE FLOW
	GUARDRAIL		FIRE HYDRANT
	10' PROPOSED CONTOURS		PROPOSED GRAVEL
	2' PROPOSED CONTOURS		PROPOSED ASPHALT PAVEMENT
	RETAINING WALL		PROPOSED SIDEWALK
	ROCKERY		PROPOSED CONCRETE
	DITCH FLOWLINE		
	STORM DRAIN PIPE		
	STORM FORCE MAIN		
	SEWER MAIN		
	SEWER FORCE MAIN		
	WATER MAIN		
	SOFT SURFACE TRAIL PATH		

WHEN A ROADWAY IS ADJACENT TO A WETLAND, GEOTECHNICAL ENGINEER REVIEW SHALL BE REQUIRED DURING THE UTILITY CONSTRUCTION TO ENSURE THAT ANY ADJACENT PROXIMATE WETLAND DOES NOT DEWATER THROUGH THE FREE-DRAINING UTILITY TRENCH BACKFILL.

TRACT DESIGNATION LEGEND

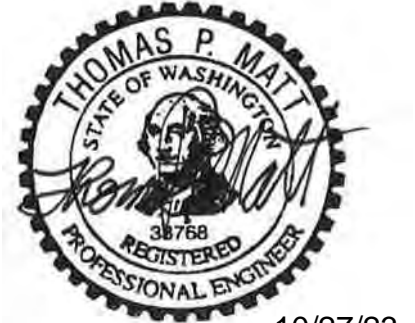
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SEE SHEET 'RD1' FOR PROPOSED ROAD SECTIONS

REVIEWED BY:	DATE:	BY:
DATE	REVISION	OK
NO.	NO.	DATE
1	06/27/23	DAVID M.
2	06/27/23	DAVID M.
3	10/27/23	DAVID M.

PRELIMINARY



CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP  
FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.

RS2



REVIEWED BY:	DATE:	BY:	OK:
DATE:	10/27/23	DATE:	10/27/23
REVISIONS:	1	DATE:	10/27/23
REVISIONS:	2	DATE:	10/27/23
REVISIONS:	3	DATE:	10/27/23

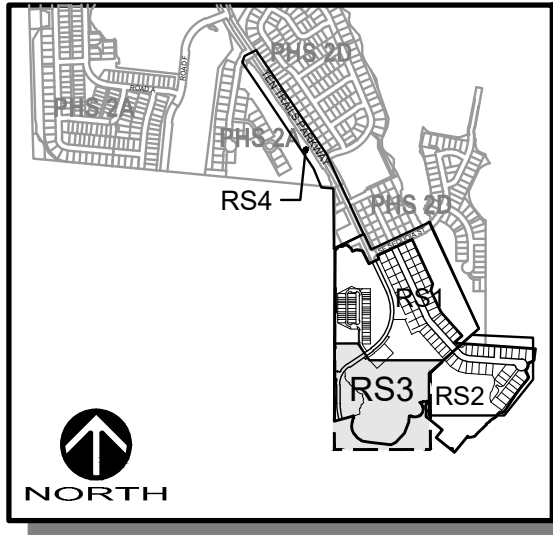


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DESIGNED BY: JAP  
DRAWN BY: JAP

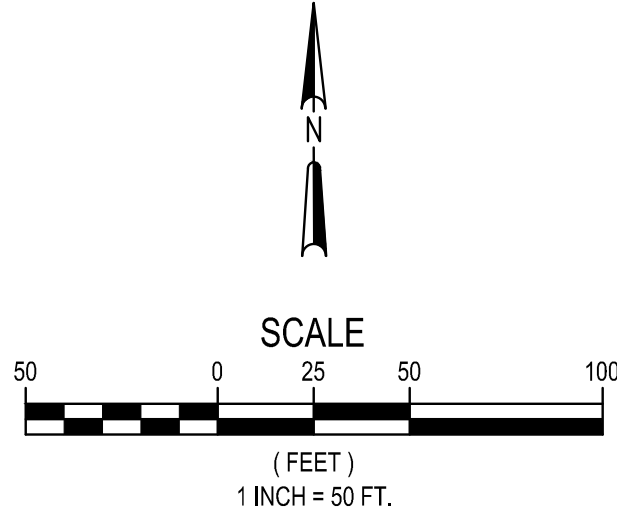
FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
RS3

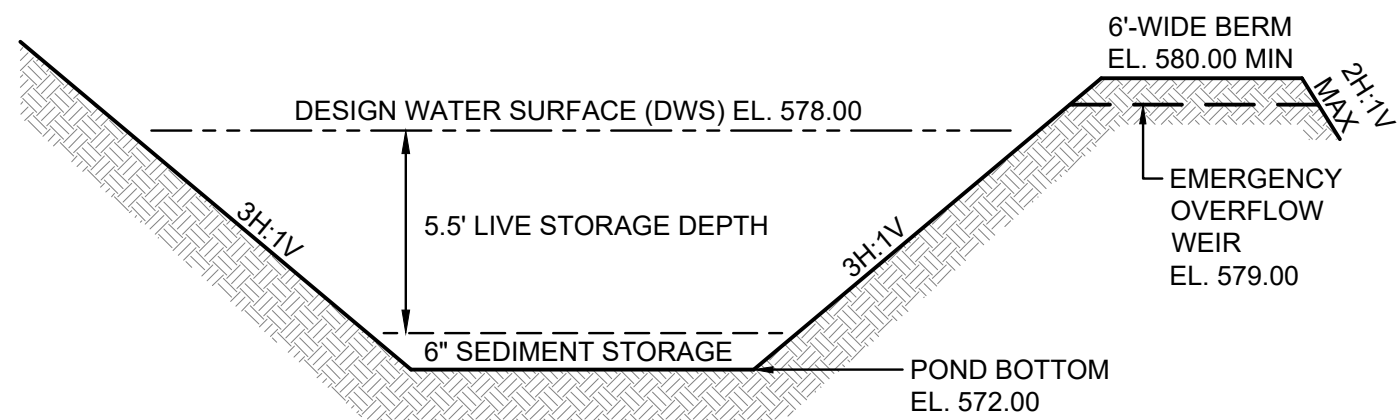


KEY MAP  
NOT TO SCALE



SEE SHEET RS2

SEE SHEET RS1



DETENTION POND SECTION  
NOT TO SCALE

TRACT DESIGNATION LEGEND

AC - ACCESS  
LA - LANDSCAPE  
NL - NATURAL LANDSCAPE  
PA - PEDESTRIAN ACCESS  
PK - PARK  
SA - SENSITIVE AREA AND BUFFER  
UT - UTILITY  
FD - FUTURE DEVELOPMENT

NOTES:  
ALL ROAD SECTIONS TO BE PER THE TEN TRAILS (F.K.A. VILLAGES) MPD DEVELOPMENT AGREEMENT.

THE UTILITY LINE TYPES AND LOCATIONS SHOULD BE CONSIDERED SCHEMATIC AND ARE REPRESENTATIVE OF THE GENERAL PRESENCE OF THE UTILITY ONLY. THE LOCATION AND CONFIGURATION OF UTILITY PIPES IS NOT APPROVED IN THIS APPLICATION.

SEE SHEET 'RD1' FOR PROPOSED ROAD SECTIONS

LEGEND

ROAD, UTILITIES AND SITE

BOUNDARY	CURB RAMP
RIGHT-OF-WAY	CURB RAMP (FLARED)
LOT LINE	CATCH BASIN, TYPE I
GUTTER, FLOW, & CURB LINE	CATCH BASIN, TYPE II
CENTER LINE	OVERFLOW STRUCTURE
EASEMENT	PIPE FLOW
BUILDING SETBACK LINE (BSBL)	INTAKE OR OUTFALL
EDGE OF WETLAND	SEWER MANHOLE
BUFFER LINE	PIPE FLOW
GUARDRAIL	FIRE HYDRANT
10' PROPOSED CONTOURS	PROPOSED GRAVEL
2' PROPOSED CONTOURS	PROPOSED ASPHALT PAVEMENT
RETAINING WALL	PROPOSED SIDEWALK
ROCKERY	PROPOSED CONCRETE
DITCH FLOWLINE	
SD - STORM DRAIN PIPE	
FM - STORM FORCE MAIN	
SS - SEWER MAIN	
SEWER FORCE MAIN	
W - WATER MAIN	
SOFT SURFACE TRAIL PATH	

SOUTH PIPE PENETRATION TO BE PLUGGED UNTIL REGIONAL STORMWATER MANAGEMENT FACILITY IS PERMITTED AND OPERATIONAL.

STORMWATER FORCE MAIN LAYOUT IS SCHEMATIC AND ACTUAL ALIGNMENTS WILL BE DEVELOPED IN CONJUNCTION WITH THE UTILITY PERMIT.

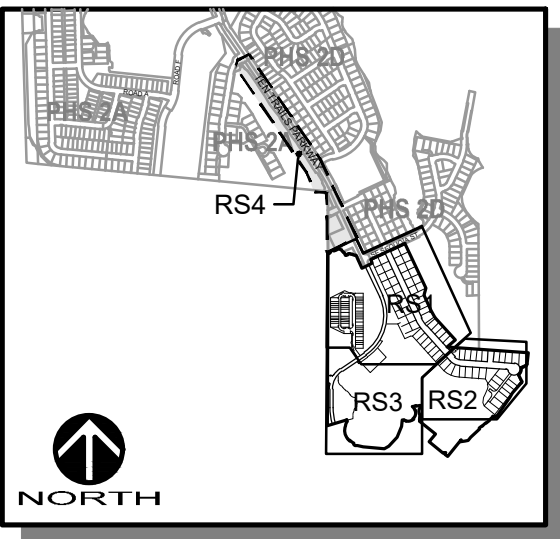
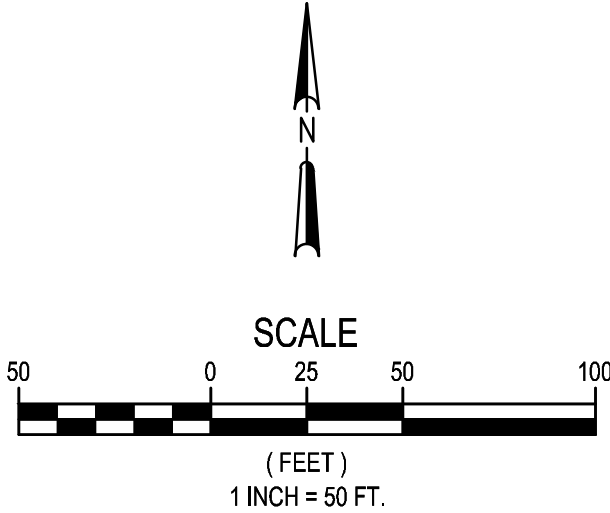
WHEN A ROADWAY IS ADJACENT TO A WETLAND, GEOTECHNICAL ENGINEER REVIEW SHALL BE REQUIRED DURING THE UTILITY CONSTRUCTION TO ENSURE THAT ANY ADJACENT PROXIMATE WETLAND DOES NOT DEWATER THROUGH THE FREE-DRAINING UTILITY TRENCH BACKFILL.



PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.

SEE LOWER LEFT

SEE UPPER RIGHT



KEY MAP  
NOT TO SCALE

PRELIMINARY PLAT  
ROAD, STORM DRAINAGE AND GRADING PLAN

TEN TRAILS  
PHASE 2 - PLAT F

WASHINGTON

CITY OF BLACK DIAMOND

REVIEWED BY:	DATE:	BY:	CHK:
DATE	REVISION	DATE	REVISION
10/27/23	1	DAVID EVANS	10/18/23
10/27/23	2	DAVID EVANS	10/18/23
10/27/23	3	DAVID EVANS	10/18/23

PRELIMINARY



10/27/23

CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
RS4

LEGEND			
ROAD, UTILITIES AND SITE			
	BOUNDARY		CURB RAMP
	RIGHT-OF-WAY		CURB RAMP (FLARED)
	LOT LINE		CATCH BASIN, TYPE I
	GUTTER, FLOW, & CURB LINE		CATCH BASIN, TYPE II
	CENTER LINE		OVERFLOW STRUCTURE
	EASEMENT		PIPE FLOW
	BUILDING SETBACK LINE (BSBL)		INTAKE OR OUTFALL
	EDGE OF WETLAND		SEWER MANHOLE
	BUFFER LINE		PIPE FLOW
	GUARDRAIL		FIRE HYDRANT
	10' PROPOSED CONTOURS		PROPOSED GRAVEL
	2' PROPOSED CONTOURS		PROPOSED ASPHALT PAVEMENT
	RETAINING WALL		PROPOSED SIDEWALK
	ROCKERY		PROPOSED CONCRETE
	DITCH FLOWLINE		
	STORM DRAIN PIPE		
	STORM FORCE MAIN		
	SEWER MAIN		
	SEWER FORCE MAIN		
	WATER MAIN		
	SOFT SURFACE TRAIL PATH		

SDFM CONFIGURATION IS SCHEMATIC AND ACTUAL ALIGNMENTS WILL BE DEVELOPED IN CONJUNCTION WITH THE UTILITY PERMIT.

DISCHARGE SDFM INTO EX. CB 3  
EX. CB 3, TYPE 2-48" RIM 575.5  
IE 572.40 (12" OUT-NW)

8" SDFM 3,227 LF

8" SDFM 3,227 LF

WETLAND P/Q CATEGORY III 60' BUFFER

PHASE 2 PLAT A

TRACT I UT/LA/PA SA/UT/PA (PLAT 2D)

TRACT J AC/UT/PA/LA (PLAT 2D)

8" SDFM 3,227 LF

WETLAND 216 CATEGORY III 60' BUFFER

TRACT K SA/UT/PA (PLAT 2D)

TRACT L PK/LA/UT/PA (PLAT 2D)

SEE SHEET RS1

STORMWATER FORCE MAIN LAYOUT IS SCHEMATIC AND ACTUAL ALIGNMENTS WILL BE DEVELOPED IN CONJUNCTION WITH THE UTILITY PERMIT.

WHEN A ROADWAY IS ADJACENT TO A WETLAND, GEOTECHNICAL ENGINEER REVIEW SHALL BE REQUIRED DURING THE UTILITY CONSTRUCTION TO ENSURE THAT ANY ADJACENT PROXIMATE WETLAND DOES NOT DEWATER THROUGH THE FREE-DRAINING UTILITY TRENCH BACKFILL.

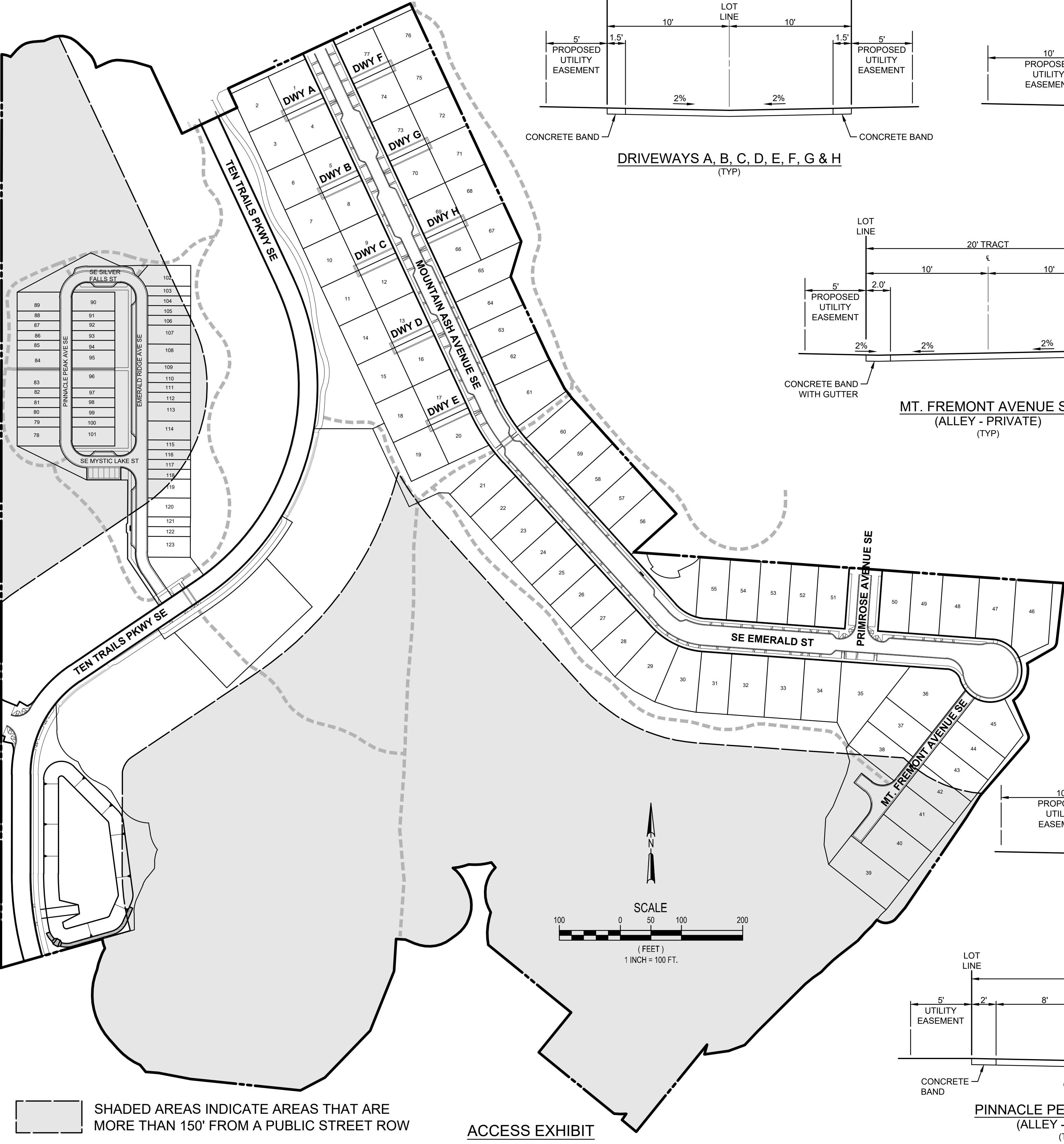
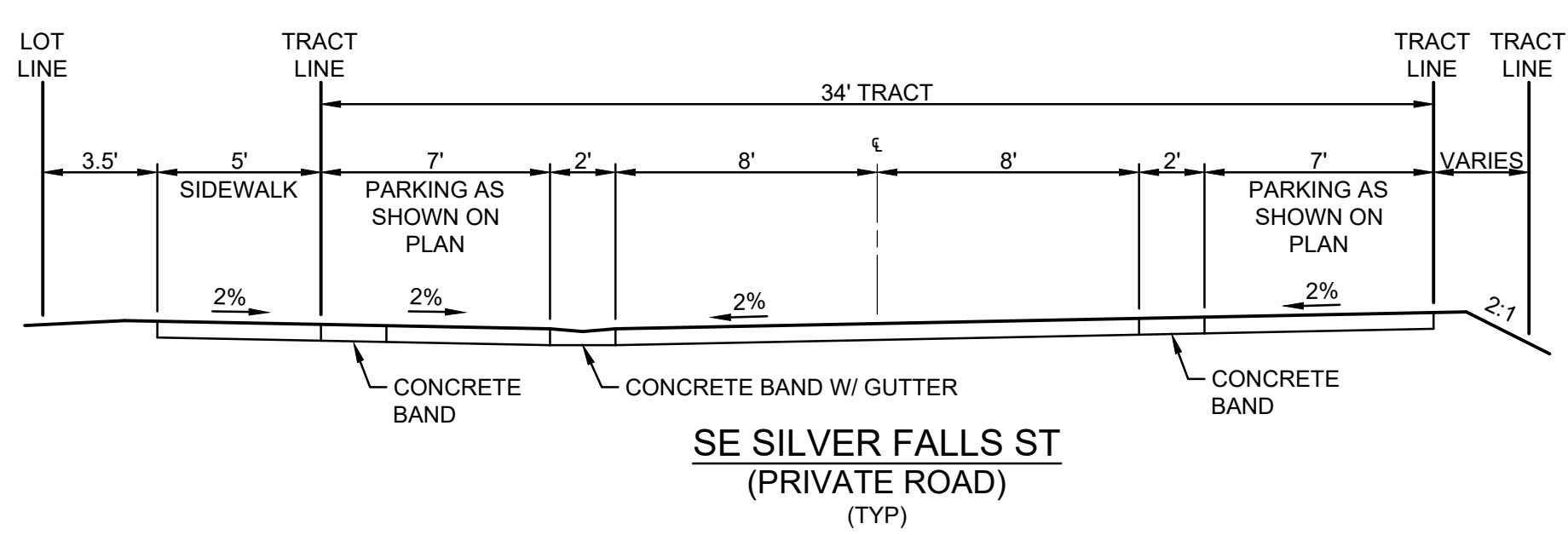
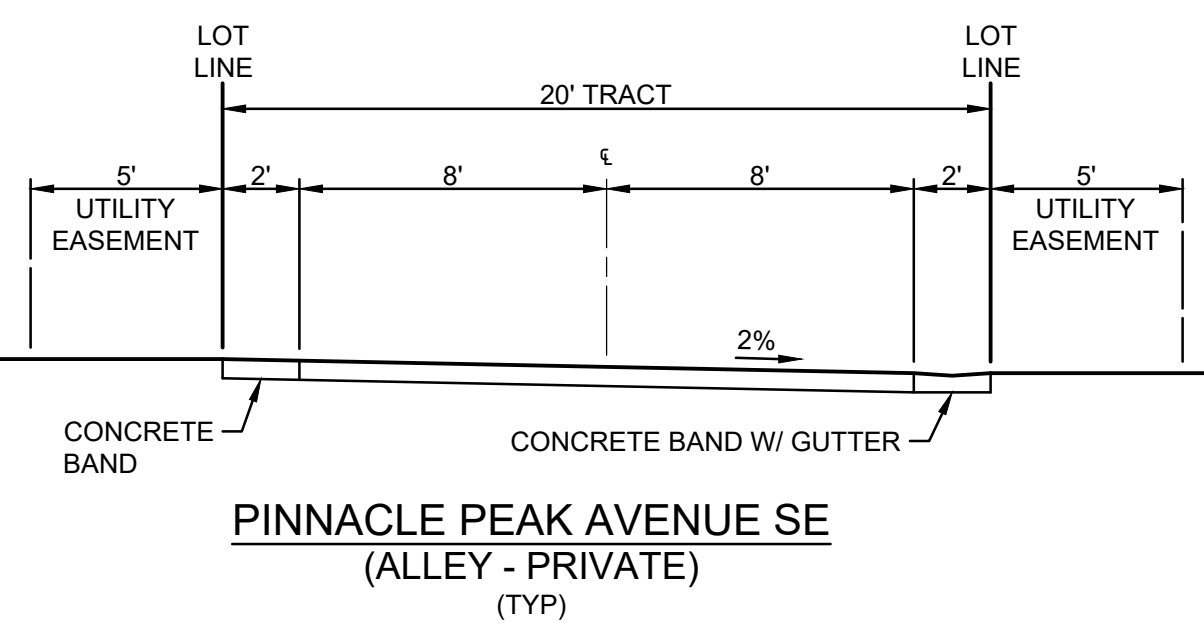
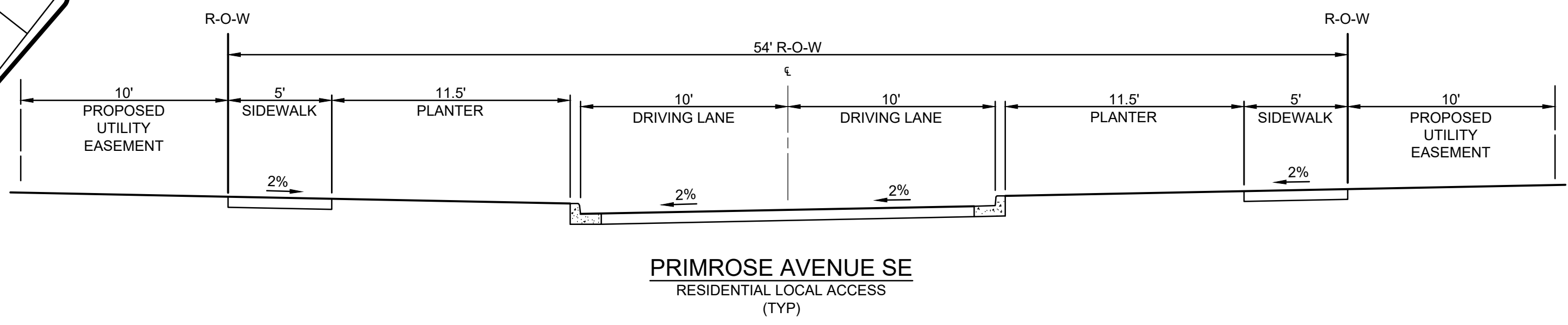
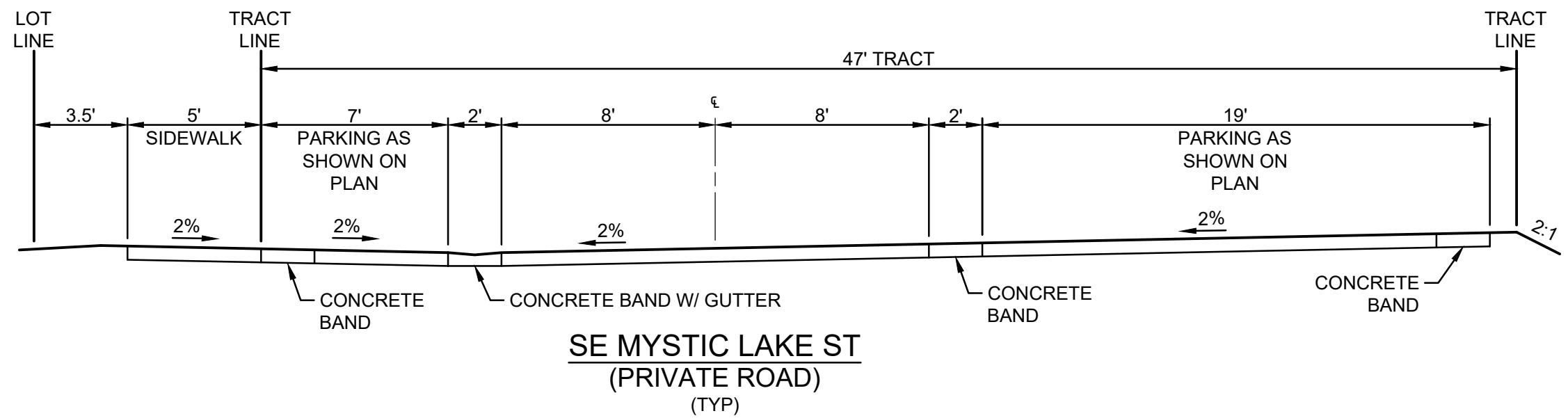
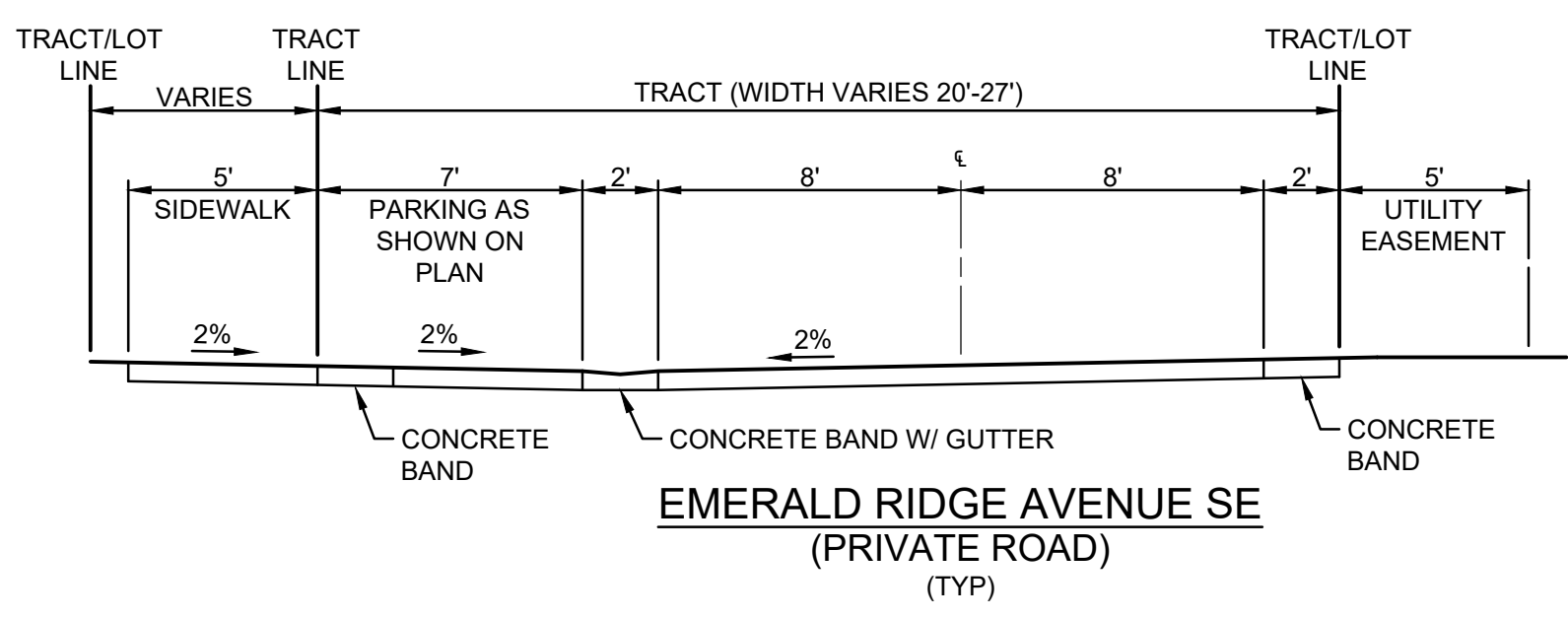
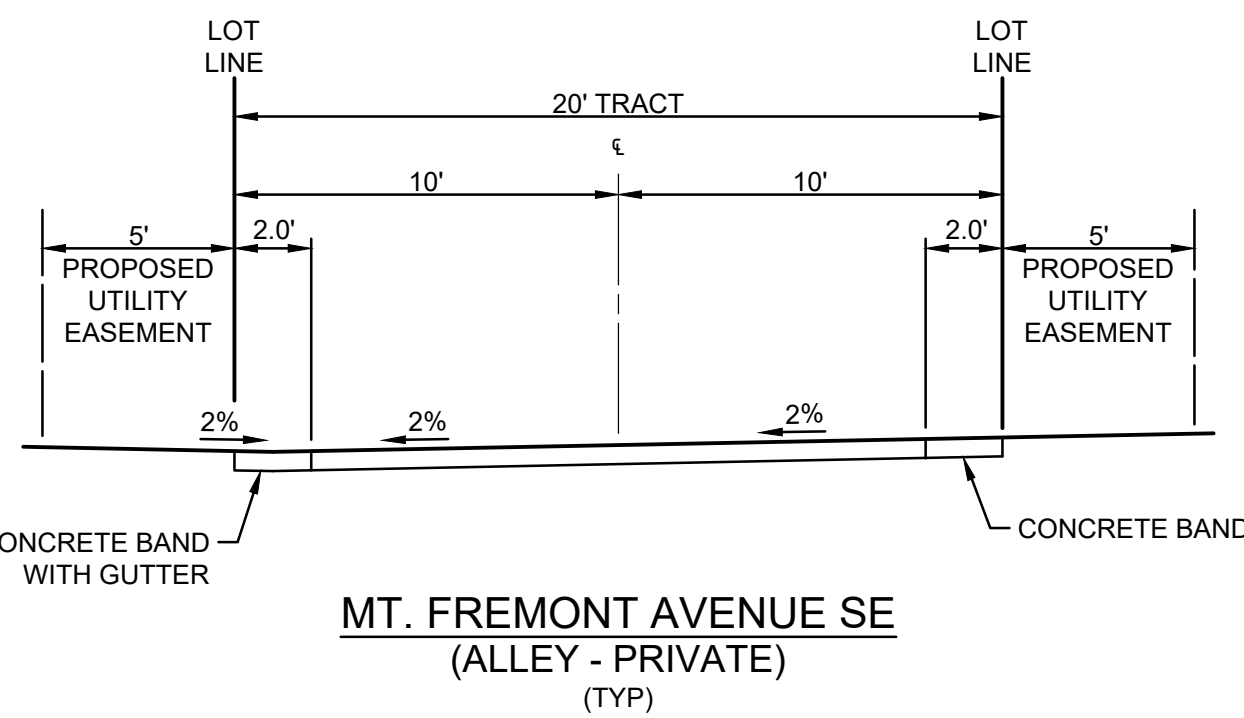
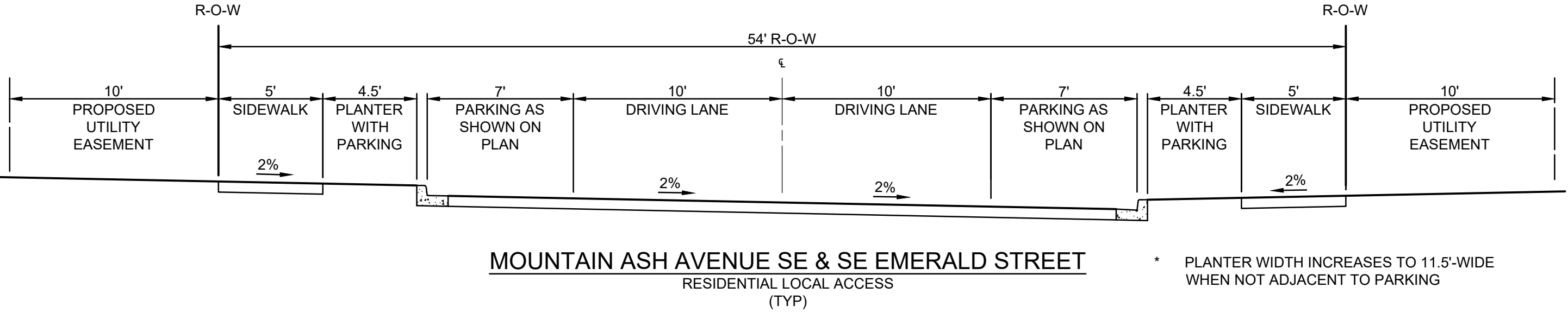
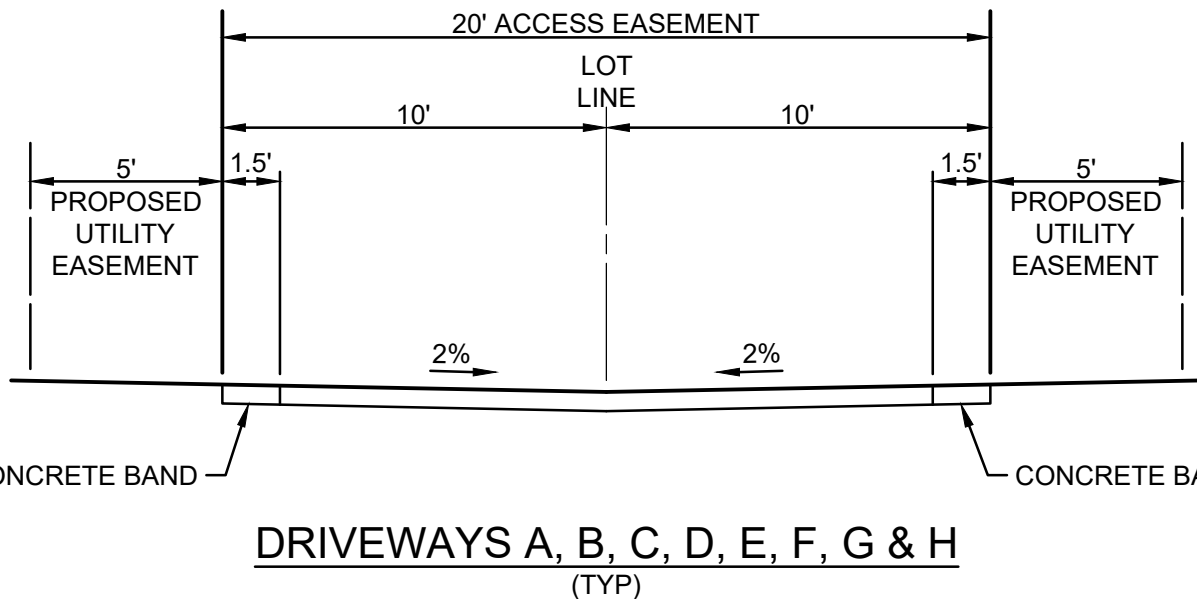
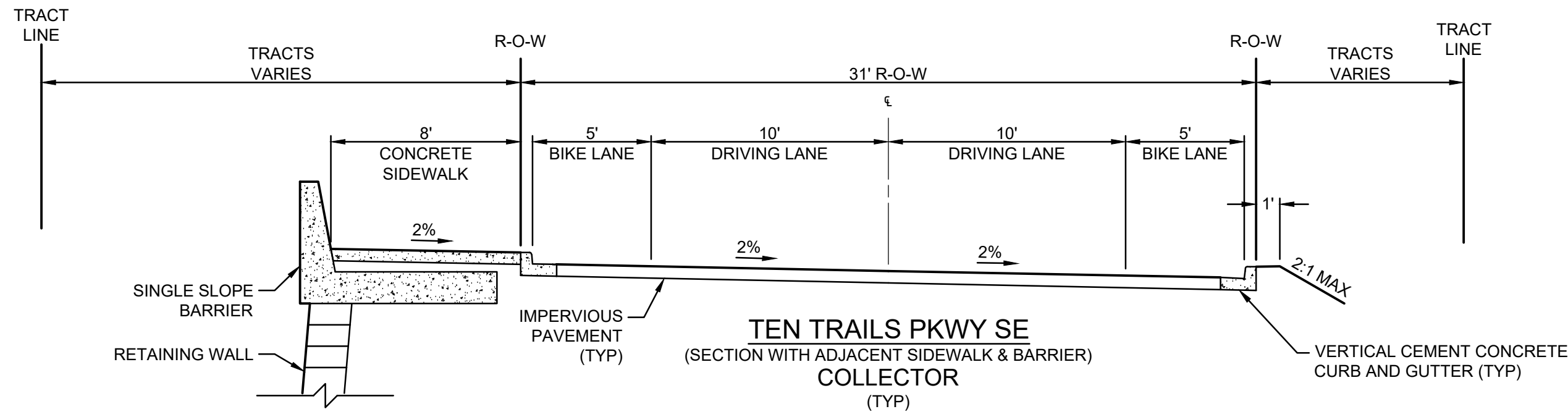
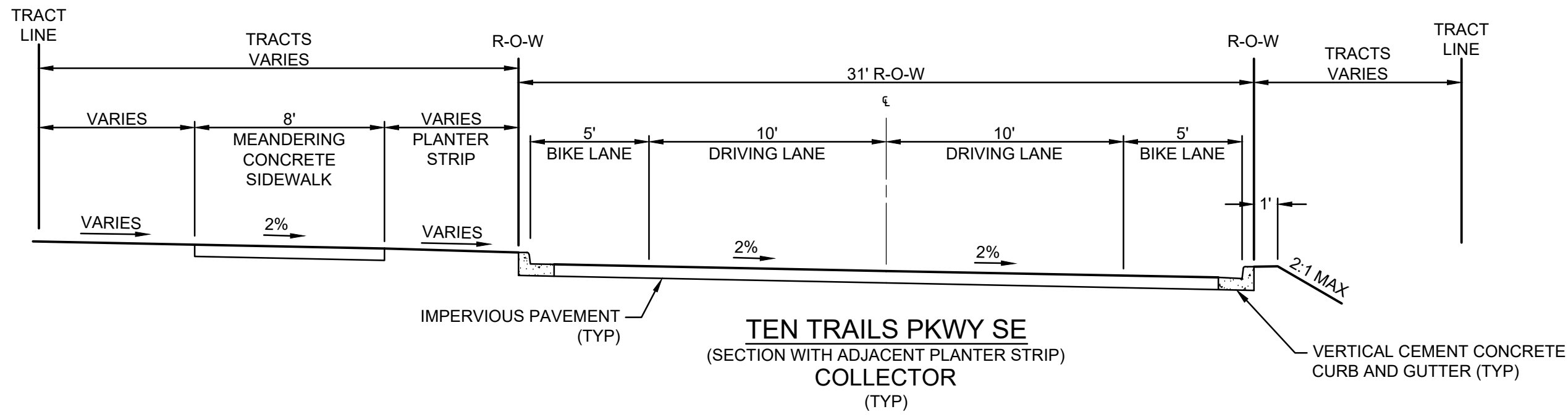
TRACT DESIGNATION LEGEND

AC - ACCESS  
LA - LANDSCAPE  
NL - NATURAL LANDSCAPE  
PA - PEDESTRIAN ACCESS  
PK - PARK  
SA - SENSITIVE AREA AND BUFFER  
UT - UTILITY  
FD - FUTURE DEVELOPMENT

NOTES:  
ALL ROAD SECTIONS TO BE PER THE TEN TRAILS (F.K.A. VILLAGES) MPD DEVELOPMENT AGREEMENT.  
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PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.

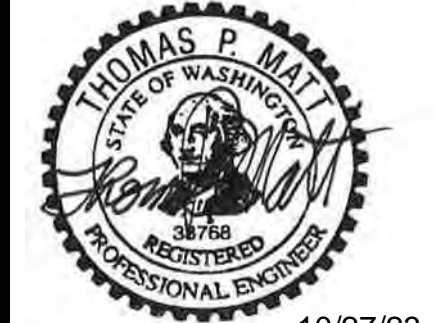


SHADED AREAS INDICATE AREAS THAT ARE MORE THAN 150' FROM A PUBLIC STREET ROW

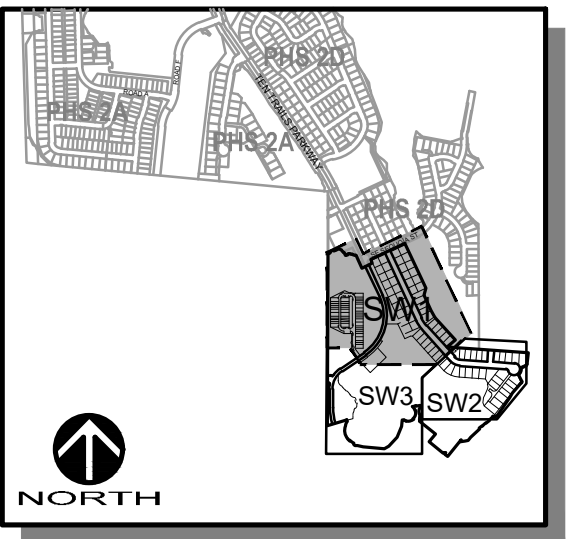
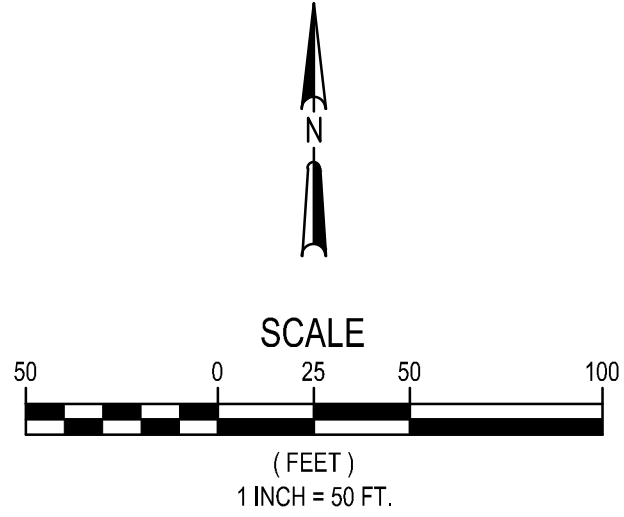
ACCESS EXHIBIT

REVIEWED BY:	DATE:	BY:
1 DATE	10/27/23	OK
2 REVISIONS	10/27/23	OK
3 CLIENT REVISIONS	10/27/23	OK
4 PER CITY REVIEW COMMENTS DATED 10/18/23	10/27/23	OK

PRELIMINARY







REVIEWED BY:	DATE:	BY:	OK:
DATE:	REVISION:	DATE:	REVISION:
NO. 1	8/2/23	CLIENT REVISIONS	DATE/TIME
2	10/27/23	PER CITY REVIEW COMMENTS DATED 10/18/23	DATE/TIME
3			



10/27/23

CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.

SW1

LEGEND	
ROAD, UTILITIES AND SITE	
BOUNDARY	CURB RAMP
RIGHT-OF-WAY	CURB RAMP (FLARED)
LOT LINE	CATCH BASIN, TYPE I
GUTTER, FLOW, & CURB LINE	CATCH BASIN, TYPE II
CENTER LINE	OVERFLOW STRUCTURE
EASEMENT	PIPE FLOW
BUILDING SETBACK LINE (BSBL)	INTAKE OR OUTFALL
EDGE OF WETLAND	SEWER MANHOLE
BUFFER LINE	PIPE FLOW
GUARDRAIL	FIRE HYDRANT
10' PROPOSED CONTOURS	PROPOSED GRAVEL
2' PROPOSED CONTOURS	PROPOSED ASPHALT PAVEMENT
RETAINING WALL	PROPOSED SIDEWALK
ROCKERY	PROPOSED CONCRETE
DITCH FLOWLINE	
SD STORM DRAIN PIPE	
FM STORM FORCE MAIN	
SS SEWER MAIN	
SEWER FORCE MAIN	
W WATER MAIN	
SOFT SURFACE TRAIL PATH	

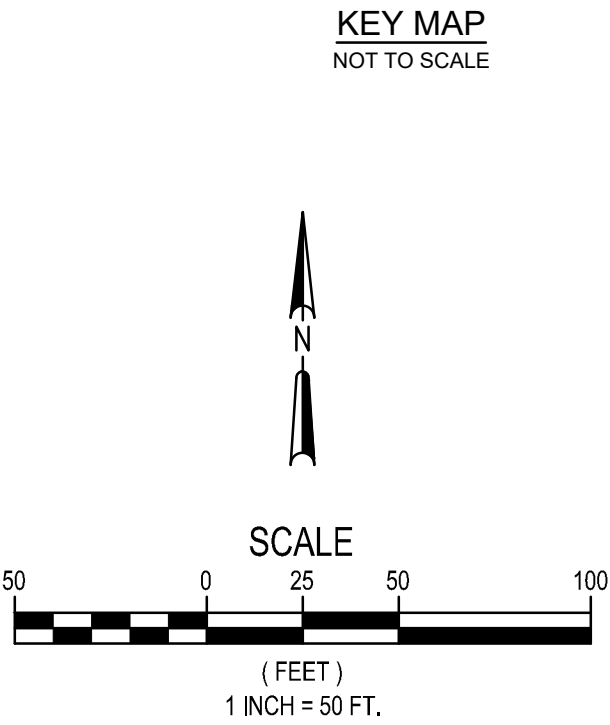
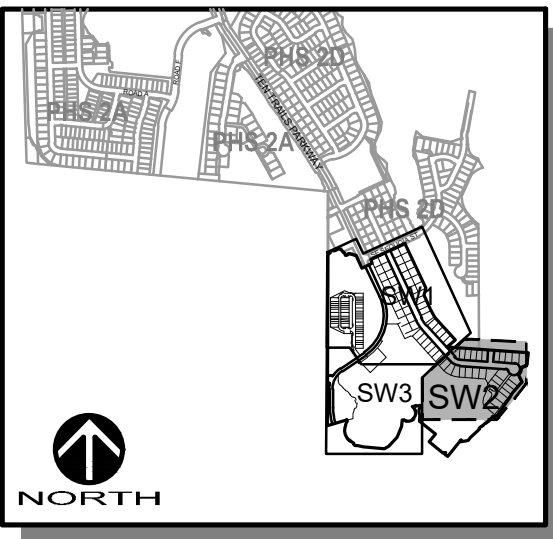
TRACT DESIGNATION LEGEND

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LA - LANDSCAPE  
NL - NATURAL LANDSCAPE  
PA - PEDESTRIAN ACCESS  
PK - PARK  
SA - SENSITIVE AREA AND BUFFER  
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NOTES:  
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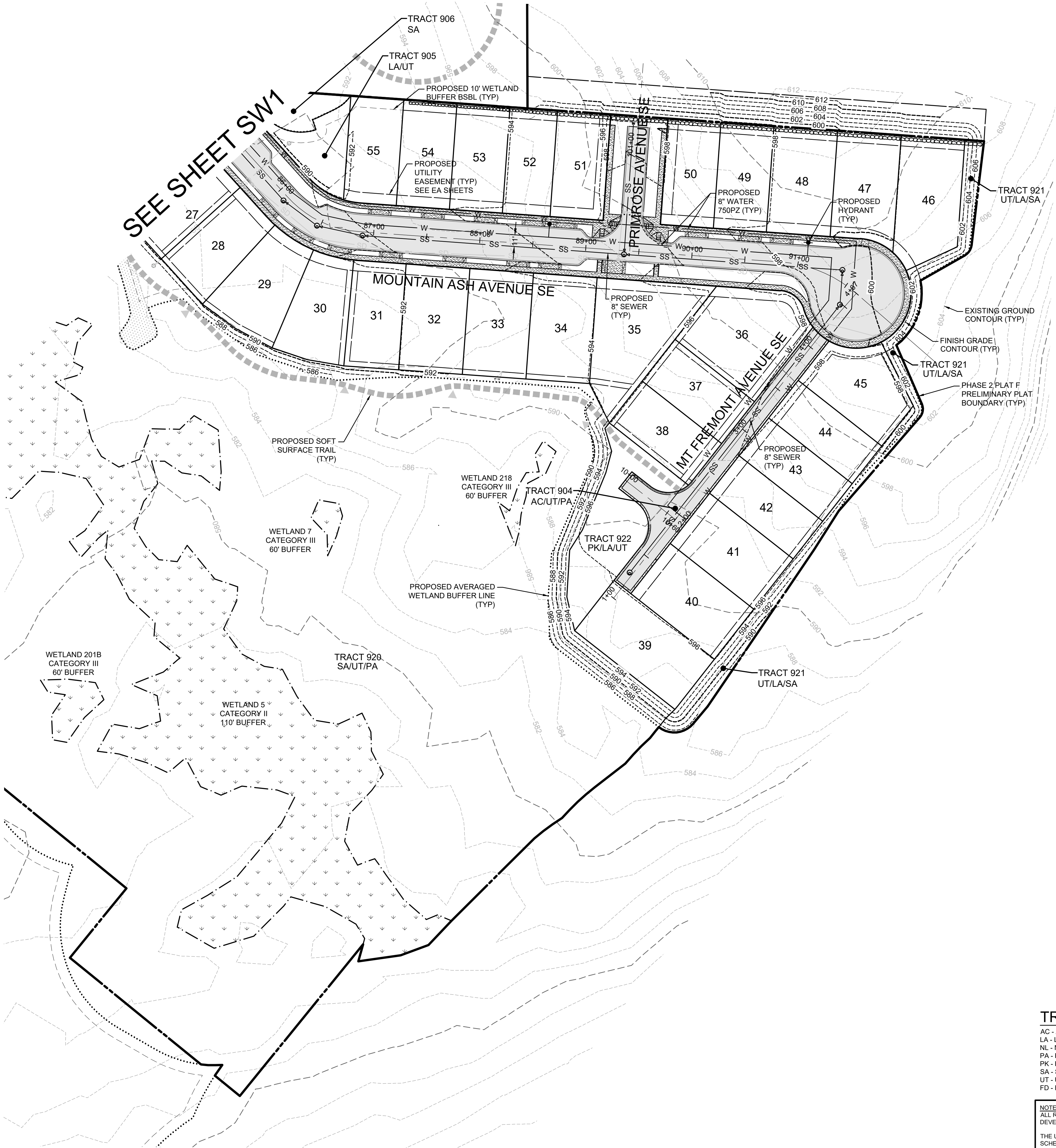
PORTION OF THE NW 1/4, SW 1/4, AND SE 1/4 OF THE NE 1/4 OF SEC. 22, TWN 21 N., RGE 6 E., W.M.



SEE SHEET SW3

SEE SHEET SW1

LEGEND			
ROAD, UTILITIES AND SITE			
	BOUNDARY		CURB RAMP
	RIGHT-OF-WAY		CURB RAMP (FLARED)
	LOT LINE		CATCH BASIN, TYPE I
	GUTTER, FLOW, & CURB LINE		CATCH BASIN, TYPE II
	CENTER LINE		OVERFLOW STRUCTURE
	EASEMENT		PIPE FLOW
	BUILDING SETBACK LINE (BSBL)		INTAKE OR OUTFALL
	EDGE OF WETLAND		SEWER MANHOLE
	BUFFER LINE		PIPE FLOW
	GUARDRAIL		FIRE HYDRANT
	10' PROPOSED CONTOURS		PROPOSED GRAVEL
	2' PROPOSED CONTOURS		PROPOSED ASPHALT PAVEMENT
	RETAINING WALL		PROPOSED SIDEWALK
	ROCKERY		PROPOSED CONCRETE
	DITCH FLOWLINE		
	SD - STORM DRAIN PIPE		
	FM - STORM FORCE MAIN		
	SS - SEWER MAIN		
	SEWER FORCE MAIN		
	W - WATER MAIN		
	SOFT SURFACE TRAIL PATH		



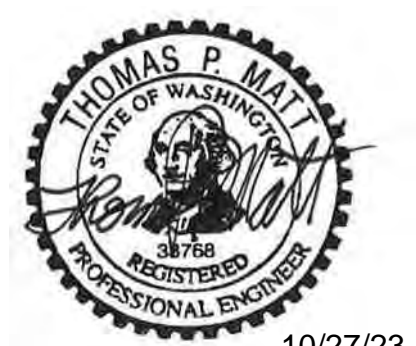
#### TRACT DESIGNATION LEGEND

AC - ACCESS  
LA - LANDSCAPE  
NL - NATURAL LANDSCAPE  
PA - PEDESTRIAN ACCESS  
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REVIEWED BY:	DATE:	BY:	CHK:
1	DATE	DATE	DATE
2	REVISION	REVISION	REVISION
3	CLIENT COMMENTS	CLIENT COMMENTS	CLIENT COMMENTS
4	PER CITY REVIEW COMMENTS DATED 10/18/23	PER CITY REVIEW COMMENTS DATED 10/18/23	PER CITY REVIEW COMMENTS DATED 10/18/23

#### PRELIMINARY

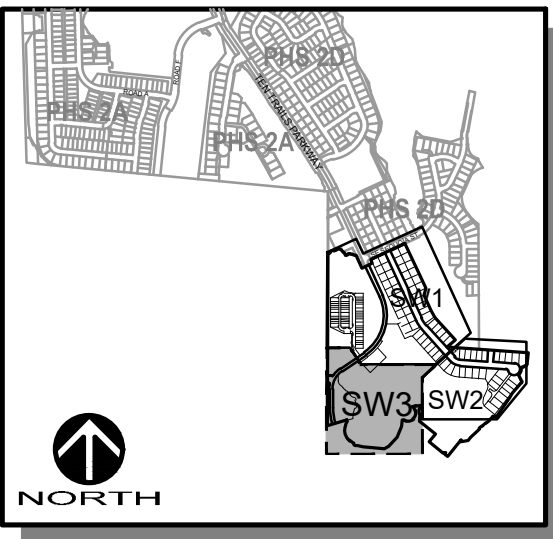


CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP  
FIRST SUBMITTAL DATE: 3/8/23

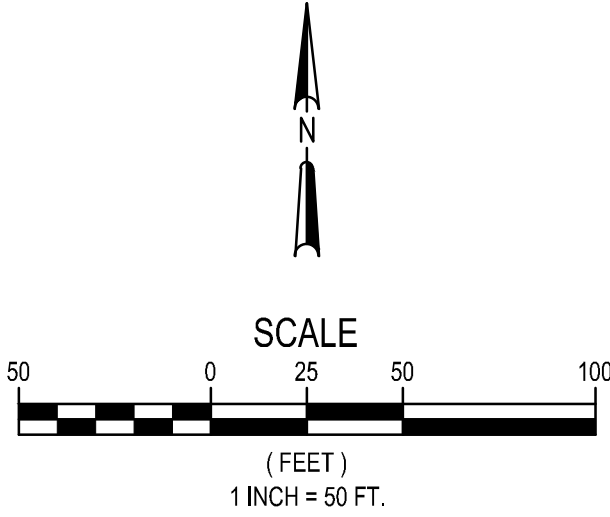
PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
SW2

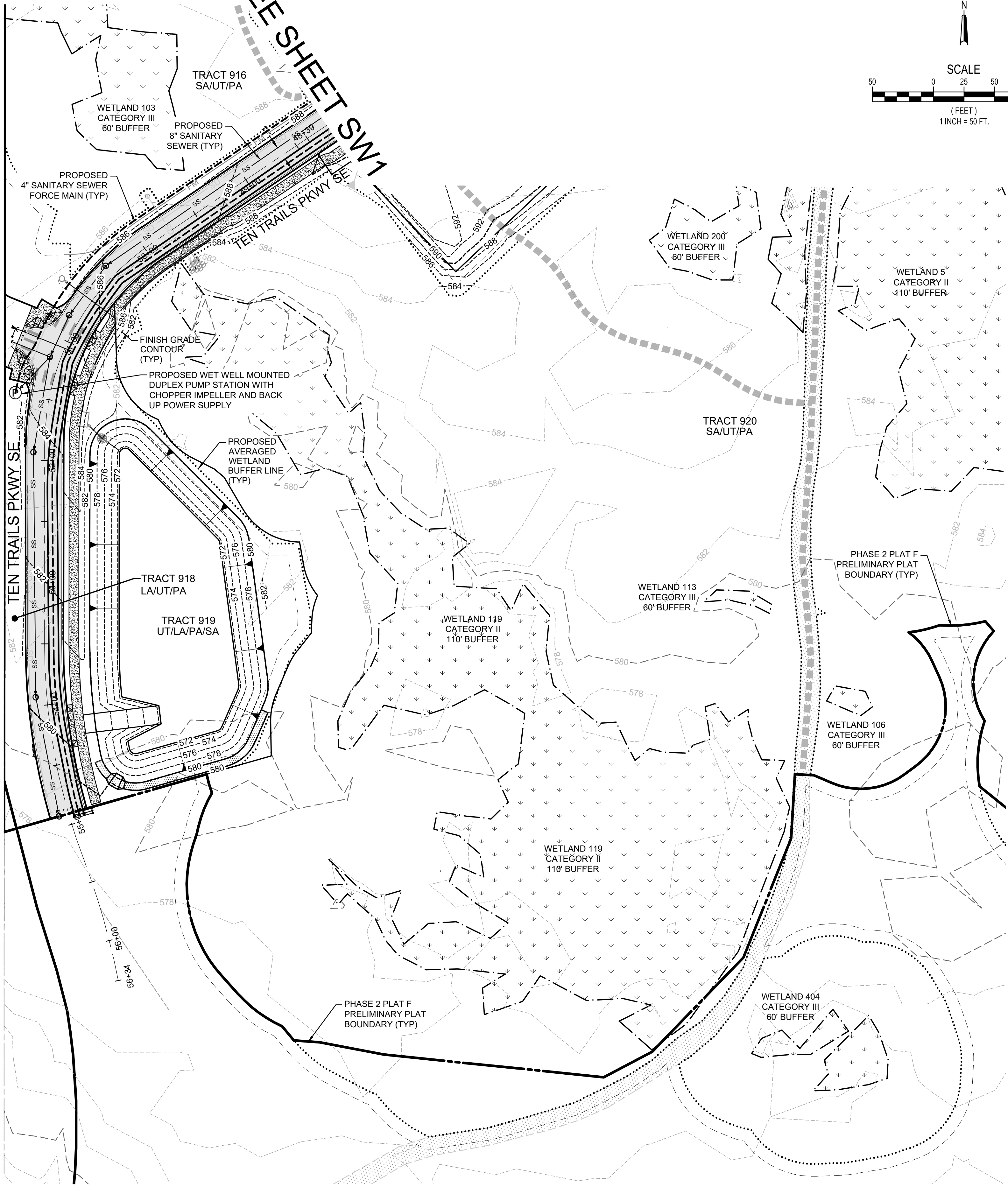




KEY MAP  
NOT TO SCALE



SEE SHEET SW1



### TRACT DESIGNATION LEGEND

AC - ACCESS  
LA - LANDSCAPE  
NL - NATURAL LANDSCAPE  
PA - PEDESTRIAN ACCESS  
PK - PARK  
SA - SENSITIVE AREA AND BUFFER  
UT - UTILITY  
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NOTES:  
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### LEGEND

#### ROAD, UTILITIES AND SITE

-----	BOUNDARY	▣	CURB RAMP
=====	RIGHT-OF-WAY	▤	CURB RAMP (FLARED)
=====	LOT LINE	■	CATCH BASIN, TYPE I
=====	GUTTER, FLOW, & CURB LINE	●	CATCH BASIN, TYPE II
-----	CENTER LINE	○	OVERFLOW STRUCTURE
-----	EASEMENT	▸	PIPE FLOW
-----	BUILDING SETBACK LINE (BSBL)	⋈	INTAKE OR OUTFALL
-----	EDGE OF WETLAND	○	SEWER MANHOLE
-----	BUFFER LINE	▸	PIPE FLOW
-----	GUARDRAIL	●	FIRE HYDRANT
-----420-----	10' PROPOSED CONTOURS	▣	PROPOSED GRAVEL
-----422-----	2' PROPOSED CONTOURS	▣	PROPOSED ASPHALT PAVEMENT
=====	RETAINING WALL	▣	PROPOSED SIDEWALK
=====	ROCKERY	▣	PROPOSED CONCRETE
-----	DITCH FLOWLINE		
-----SD-----	STORM DRAIN PIPE		
-----FM-----	STORM FORCE MAIN		
-----SS-----	SEWER MAIN		
-----	SEWER FORCE MAIN		
-----W-----	WATER MAIN		
=====	SOFT SURFACE TRAIL PATH		

### PRELIMINARY PLAT SANITARY SEWER AND WATER PLAN

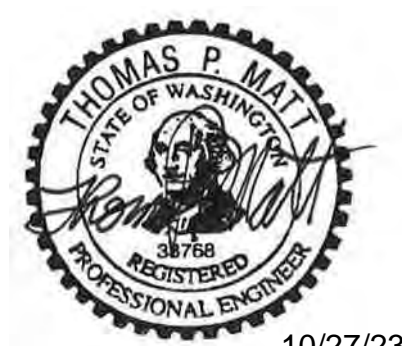
### TEN TRAILS PHASE 2 - PLAT F

WASHINGTON

CITY OF BLACK DIAMOND

REVIEWED BY:	DATE:	BY:	CHK:
DATE:	REVISION	DATE:	CHK:
NO. 1	06/23/23	DATE:	CHK:
2	06/23/23	DATE:	CHK:
3	10/27/23	DATE:	CHK:

### PRELIMINARY



10/27/23

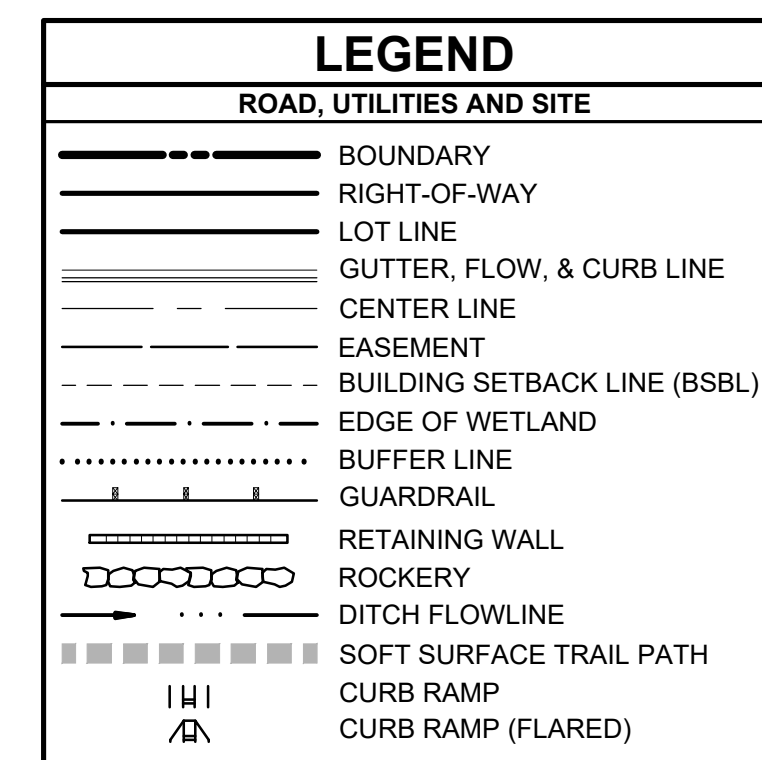
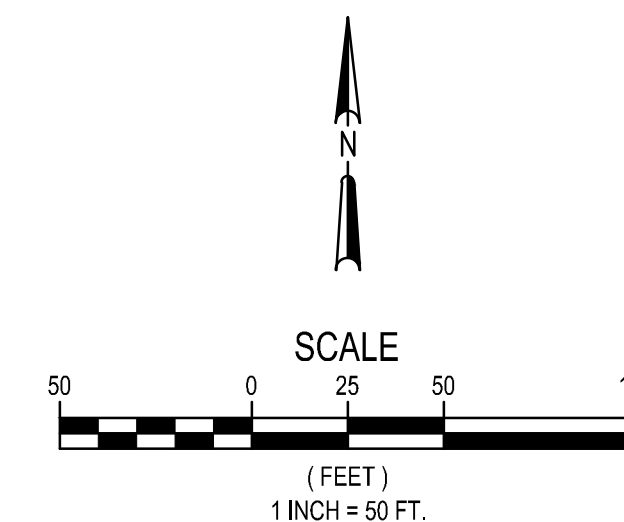
CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
SW3





EASEMENT TABLE	
#	DESCRIPTION
1	5' PRIVATE DRAINAGE EASEMENT
2	10' PRIVATE DRAINAGE EASEMENT
5	10' PUBLIC UTILITY EASEMENT
7	5' PROPOSED UTILITY EASEMENT
8	20' PROPOSED ACCESS EASEMENT
10	6' PEDESTRIAN ACCESS EASEMENT
13	10' PUBLIC AND PRIVATE UTILITY EASEMENT

AC - ACCESS  
LA - LANDSCAPE  
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PA - PEDESTRIAN ACCESS  
PK - PARK  
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**NOTES:**  
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**PRELIMINARY PLAT  
EASEMENT PLAN**

**TEN TRAILS  
PHASE 2 - PLATF**

CITY OF BLACK DIAMOND

REVIEWED BY:		DATE:	
NO.	DATE	REVISION	BY / CK
1	8/21/23	CLIENT REVISIONS	DAMTPM
2	9/6/23	OWNER/CLIENT REVISIONS	DAMTPM
3	10/27/23	PER CITY REVIEW COMMENTS DATED 10/18/23	KNIPTPM

# PRELIMINARY



CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

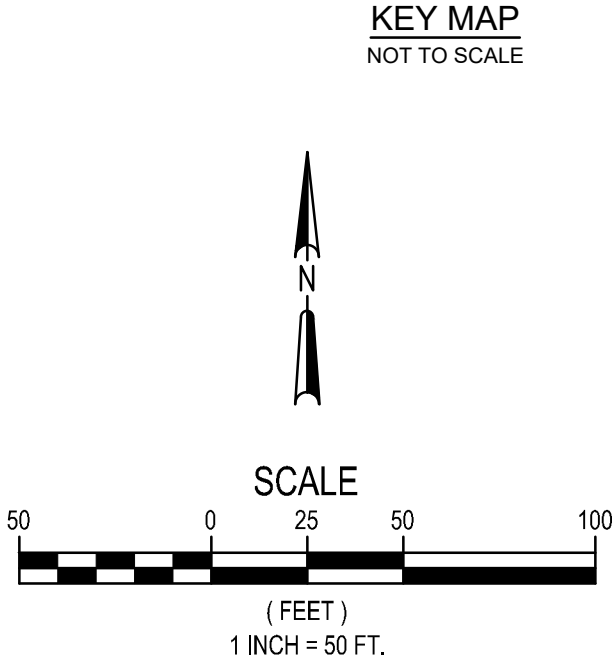
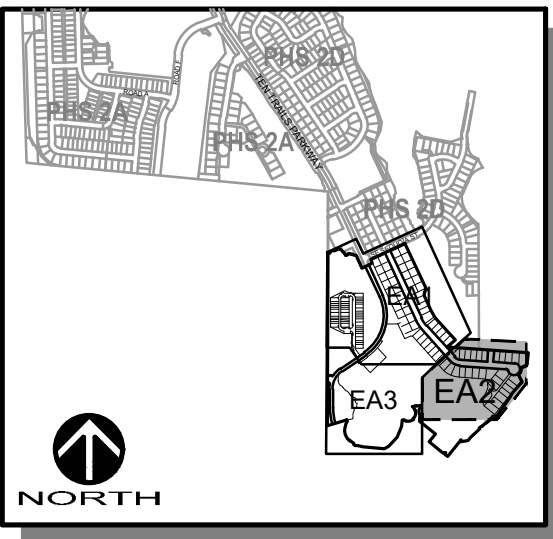
FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
**OAKPCBDP6001**

SHEET NO.

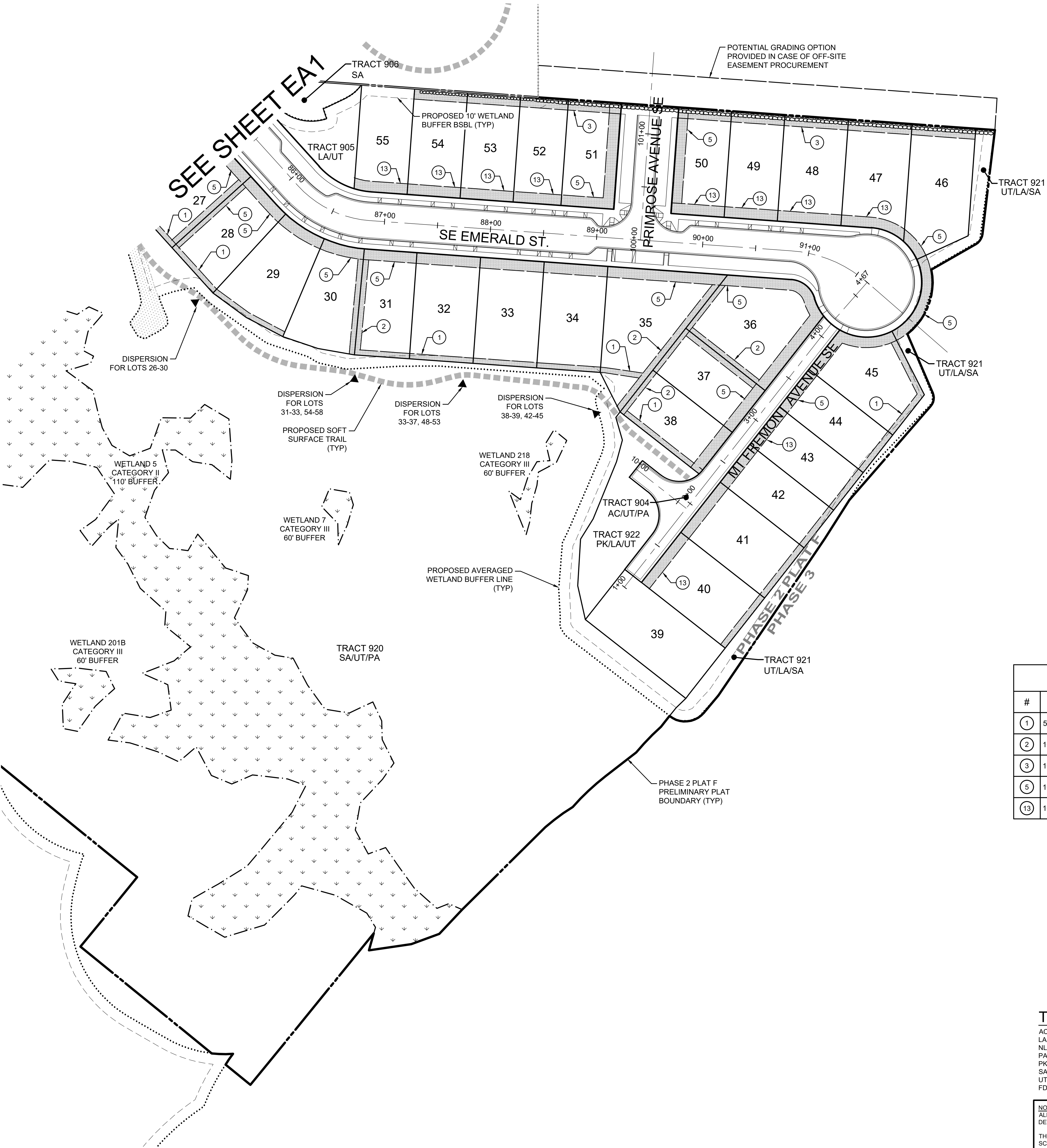
# EA1





SEE SHEET EA3

SEE SHEET EA1



EASEMENT TABLE	
#	DESCRIPTION
①	5' PRIVATE DRAINAGE EASEMENT
②	10' PRIVATE DRAINAGE EASEMENT
③	10' PRIVATE WALL AND DRAINAGE EASEMENT
⑤	10' PUBLIC UTILITY EASEMENT
⑬	10' PUBLIC AND PRIVATE UTILITY EASEMENT

TRACT DESIGNATION LEGEND

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LEGEND	
ROAD, UTILITIES AND SITE	
	BOUNDARY
	RIGHT-OF-WAY
	LOT LINE
	GUTTER, FLOW, & CURB LINE
	CENTER LINE
	EASEMENT
	BUILDING SETBACK LINE (BSBL)
	EDGE OF WETLAND
	BUFFER LINE
	GUARDRAIL
	RETAINING WALL
	ROCKERY
	DITCH FLOWLINE
	SOFT SURFACE TRAIL PATH
	CURB RAMP
	CURB RAMP (FLARED)

DATE	BY	REVIEWED BY
10/18/23	DAVID M. JAP	DAVID M. JAP
10/18/23	DAVID M. JAP	DAVID M. JAP
10/18/23	DAVID M. JAP	DAVID M. JAP



CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP  
FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
EA2



Plot Date: 10/27/2023 12:46 PM By: Kevin Puckett  
Save Date: 10/27/2023 12:24 PM By: kmpu File: P:\OAKPCBDP6001\0400CAD\CD\PRELIMINARY\Ten\_Trails\Plat\_2F\Sheets\Preliminary Plat\EA-P-OAKPCBDP6001-P2F.dwg

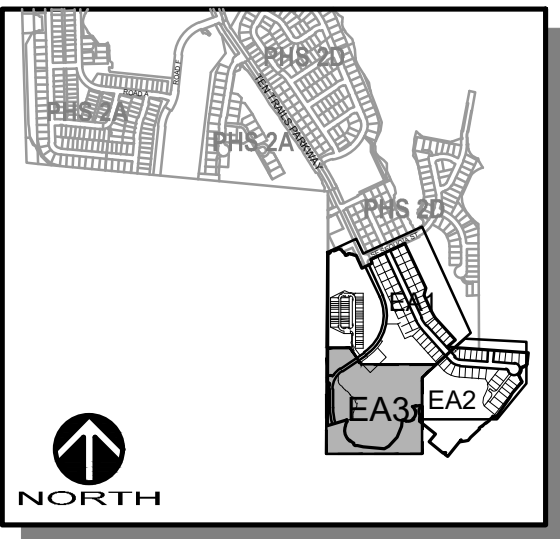
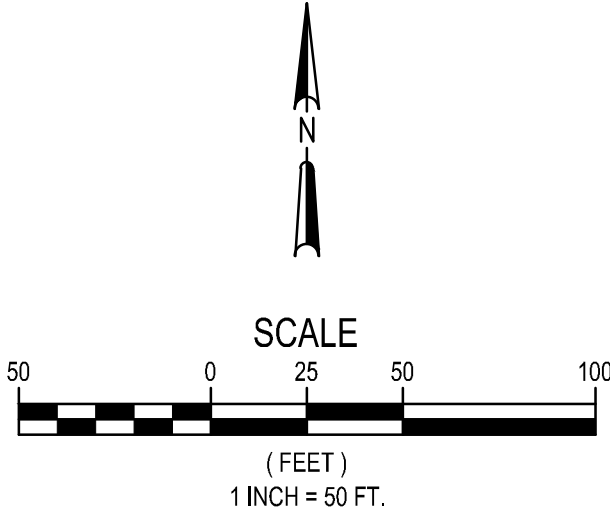
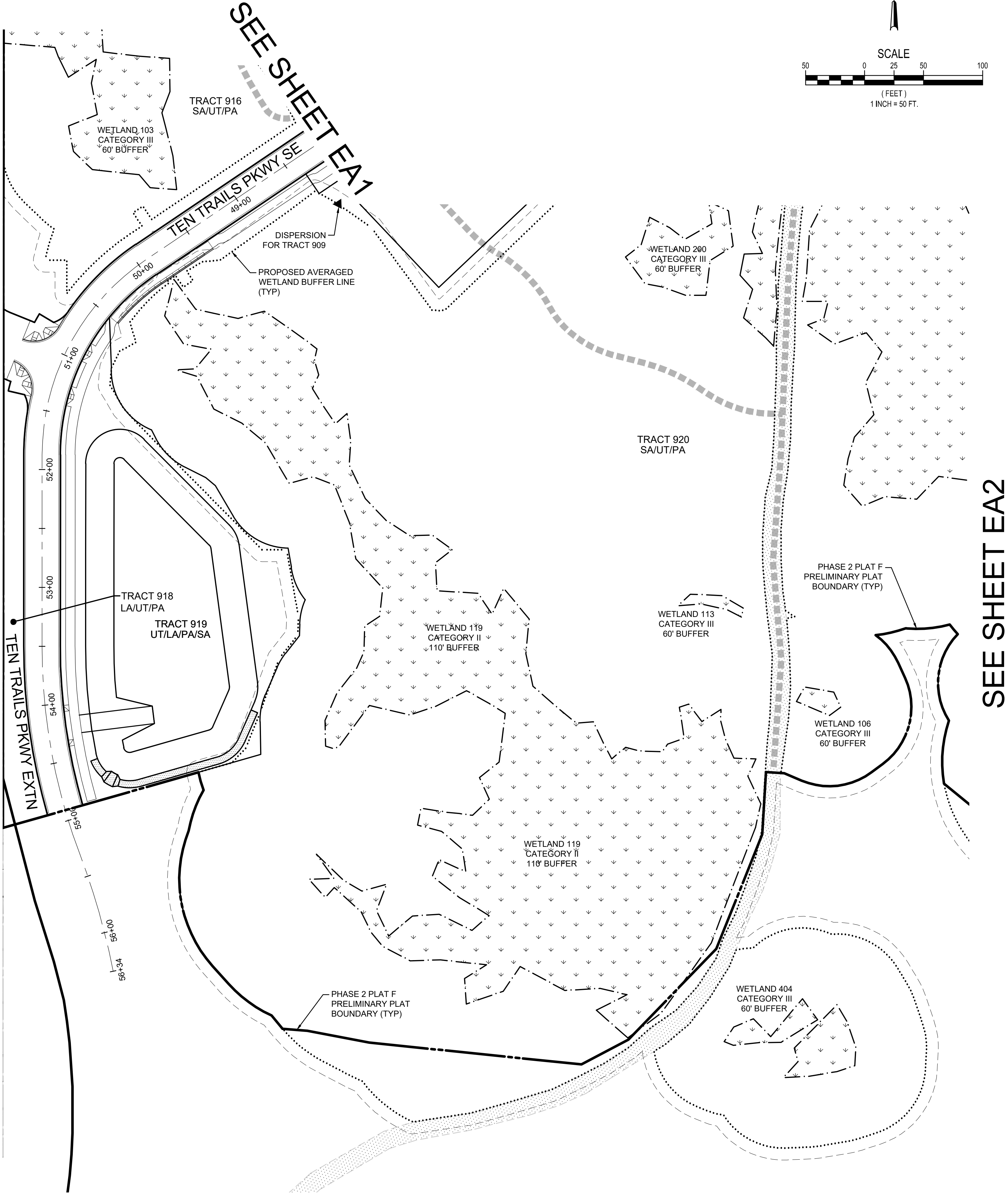
TRACT DESIGNATION LEGEND

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SA - SENSITIVE AREA AND BUFFER  
UT - UTILITY  
FD - FUTURE DEVELOPMENT

NOTES:  
ALL ROAD SECTIONS TO BE PER THE TEN TRAILS (F.K.A. VILLAGES) MPD DEVELOPMENT AGREEMENT.

THE UTILITY LINE TYPES AND LOCATIONS SHOULD BE CONSIDERED SCHEMATIC AND ARE REPRESENTATIVE OF THE GENERAL PRESENCE OF THE UTILITY ONLY. THE LOCATION AND CONFIGURATION OF UTILITY PIPES IS NOT APPROVED IN THIS APPLICATION.

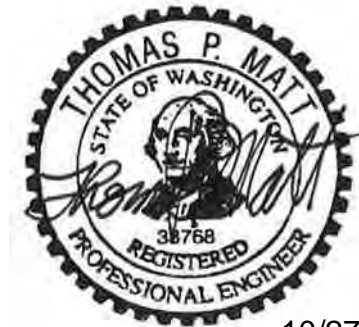
LEGEND	
ROAD, UTILITIES AND SITE	
	BOUNDARY
	RIGHT-OF-WAY
	LOT LINE
	GUTTER, FLOW, & CURB LINE
	CENTER LINE
	EASEMENT
	BUILDING SETBACK LINE (BSBL)
	EDGE OF WETLAND
	BUFFER LINE
	GUARDRAIL
	RETAINING WALL
	ROCKERY
	DITCH FLOWLINE
	SOFT SURFACE TRAIL PATH
	CURB RAMP
	CURB RAMP (FLARED)



KEY MAP  
NOT TO SCALE

REVIEWED BY:		DATE:	BY:
NO.	DATE	REVISIONS	DATE
1	8/2/23	CLIENT REVISIONS	DAV/TPM
2	8/2/23	CLIENT REVISIONS	DAV/TPM
3	10/27/23	PER CITY REVIEW COMMENTS DATED 10/18/23	KMP/TPM

PRELIMINARY



10/27/23

CHECKED BY: TPM  
DESIGNED BY: JAP  
DRAWN BY: JAP

FIRST SUBMITTAL DATE: 3/8/23

PROJECT NO.  
OAKPCBDP6001

SHEET NO.  
EA3



**TEN TRAILS MPD**

**PHASE 2 PLAT F PRELIMINARY PLAT**

**PLN23-0019**

**EXHIBIT 2**

**TITLE:** Public Comments Received During Notice of Application Period

**PREPARED BY:** Five (5) public commenters

**DATE:** Various



## Alex Campbell

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**From:** Kristen Bryant <kristenbry@gmail.com>  
**Sent:** Wednesday, May 17, 2023 6:25 PM  
**To:** Alex Campbell  
**Cc:** Alex Campbell  
**Subject:** Re: Black Diamond MPDs - Notices of Application

Thank you for the information.

This is a request, or perhaps 3 requests, for the file of documents/submittal materials / project materials related to each of the noticed applications.

Have a great day,  
Kristen Bryant

On Wed, May 17, 2023 at 9:19 AM Alex Campbell <[acampbell@blackdiamondwa.gov](mailto:acampbell@blackdiamondwa.gov)> wrote:

Hello – you are receiving this email since you have requested to be notified of permit applications associated with the Ten Trails and Lawson Hills Master Planned Developments within the City of Black Diamond. Please see the following Notices of Application attached to this email:

- PLN23-0019 Ten Trails MPD Phase 2 Plat F Preliminary Plat
- PLN23-0024 Ten Trails MPD Minor Amendment to the Development Agreement
- PLN23-0025 Lawson Hills MPD Minor Amendment to the Development Agreement

More information about these applications, including public commenting opportunities and Staff contact information, can be found in the attached documents.

Thank you,

Alex Campbell, AICP

*City of Black Diamond*

*Senior Planner, Master Development Review Team*

*360-851-4544*



## Alex Campbell

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**From:** Bernie O'Donnell <Bernie.odonnell@Rockpmservices.com>  
**Sent:** Wednesday, May 24, 2023 11:14 PM  
**To:** Alex Campbell  
**Cc:** council@blackdiamondwa.gov  
**Subject:** PLN23-0019 Ten Trails MPD – Phase 2 Plat F Preliminary Plat (Plat 2F)

**Follow Up Flag:** Flag for follow up  
**Flag Status:** Flagged

Alex,  
Please accept this public comment for the above subject preliminary plat application and consider the comments in your review and recommendation of the application and the City's Quasi-Judicial Decision:

The FEIS has specific requirements that have not yet been met by the developer. Supplemental information required as a part of this application review process should be conclusive that an additional 126 new residential lots further exacerbates the impacts to traffic and public safety in and around the Black Diamond area. Supplemental information required as a part of this application review includes a SEPA checklist and traffic study. Both supplemental information items prepared with due diligence should clearly demonstrate substantial and unacceptable impacts and therefore the application should be denied at this time and until all previous threshold agreements have been satisfied and supplemental information determinates are agreed upon and satisfactorily addressed.

Bernie O'Donnell  
Black Diamond Resident  
28935 233rd Avenue SE  
Black Diamond, WA 98010  
206.446.9800



## Alex Campbell

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**From:** Friends BlackDiamond <friendsofblackdiamond@comcast.net>  
**Sent:** Wednesday, May 31, 2023 8:45 AM  
**To:** Alex Campbell  
**Cc:** Angela Rossman Fettig; mikeyfett@live.com; Gary Davis; ladygreyreiver@comcast.net; sherriejns1@msn.com; garlicman1951@gmail.com; r.mixdog4@comcast.net; dawnis2@hotmail.com  
**Subject:** Comments Pre Plat 2F PLN23-0019/PLN23-0020  
**Attachments:** Comment Plat2F FriendsOfBlackDiamond.pdf

To Alex Campbell and the City of Black Diamond Master Developer Review Team,

Attached are comments from Friends of Black Diamond and the individuals below on the Preliminary Plat "notice of application" on the city's public notice page at <https://www.blackdiamondwa.gov/public-notices/files/noa-pln23-0019>.

The notice indicates we can "request to receive notice of the public hearing, participate in any hearing, request a copy of the decision when it becomes available, and be made aware of any appeal rights," and that there will be additional opportunities to comment. We request to be advised of all of these via the email.

Thank you,  
Friends of Black Diamond and  
Kristen Bryant  
William Bryant  
Angela Fettig  
Mike Fettig  
Gary Davis  
Lisa Winters  
Sherrie Jones  
Gary Jones  
Renee Mix



Friends of BlackDiamond  
FriendsofBlackDiamond@comcast.net

May 29, 2023

City of Black Diamond  
Master Development Review Team  
c/o Alex Campbell, acampbell@blackdiamondwa.gov  
Roberts Drive, Black Diamond WA 98010

Subject: Comments on Ten Trails Preliminary Plat 2F Application, PLN23-0020

Dear Mr. Campbell,

These are initial comments on the proposed Ten Trails Preliminary Plat 2F, regarding apparent noncompliance with applicable code and Development Agreement requirements, based on project materials provided by the city. This may not be a complete list and we may modify or submit further comments if we find additional issues or if new information becomes available.

1. The Ten Trails development, master developer OakPointe, is currently in violation of numerous provisions of the Development Agreement Ordinance 970. Therefore, new plat applications cannot be accepted or processed by the city. This Plat should be rejected.

It is self-evident that if the developer is violating provisions required as part of the overall master planned development, then a new plat governed by that master planned development agreement is not valid.

Besides the clear need for continued compliance with the Ten Trails Development Agreement in order to be allowed to construct new projects in said Development, city code requirements including the following:

“If the proposal is in an approved MPD, the proposed subdivision shall be consistent with the approved MPD, the MPD conditions of approval, the MPD design standards, and the MPD development agreement;” (BDMC 17.15.020.A.8)

And this statement requiring compliance with the MPD development agreement and that compliance be recorded on the plat:

"This plat is part of an approved Master Planned Development (MPD). All development and construction within this plat must be consistent with the applicable requirements of the MPD development agreement, MPD design standards, and mitigation requirements.” (BDMC 17.20.040.I.)

And the City’s code governing Development Agreements:



“Developer's Compliance. The development agreement shall include a clause stating that the city's duties under the agreement are expressly conditioned upon the property owner's substantial compliance with each and every term, condition, provision and/or covenant in the development agreement, all applicable federal, state and local laws and regulations and the property owner's obligations as identified in any approval or project permit for the property identified in the development agreement.” (18.66.020.D.)

The Washington state statute authorizing development agreements also does not allow subdivisions within the development if the development agreement is not complied with.

The following list of violations of the Ten Trails Development Agreement was previously provided to the city on May 4, 2023, and the details are provided later in this comment letter in the section called “Ten Trails Development Agreement Ordinance 970 violations.”

- (1) Late satellite fire station: no construction contract by 750th unit. Not under construction 1.5 years later at over 1,000 occupied units.
- (2) City athletic fields not paid for or constructed by 800 units.
- (3) Failure to meet fiscal requirement that development revenue provide sufficient funding for Fire and Police staff to serve growth.
- (4) Failure to complete Master Planned Development (MPD) Fiscal Analysis every 5 years and ensure the MPD does not have adverse financial impact.
- (5) Fixes for failing SR 169 intersections at Ravensdale Rd. and Roberts Dr. not yet permitted, but City allowing further Ten Trails occupancy in violation of Development Agreement section 11.4.
- (6) Failure to enforce Affordable Housing “balanced share” commitment in OakPointe Master Planned Developments (MPDs)

2. The preliminary plat application is not complete because it does not contain the following Black Diamond Municipal Code (BDMC) subdivision application requirements:

“17.12.010- Application. ...

G. A listing of all property owners of record within three hundred feet of the exterior boundaries of all parcels proposed to be included within the subdivision. The three hundred feet shall be measured from the exterior boundary of adjacent property in the same ownership as the land to be subdivided;

H. Certificates of sewer and water availability;



I. if the property to be subdivided has been designated as a receiving area pursuant to the city's transfer of development rights program, a copy of all development right certificates (DRCs) proposed for use in obtaining the proposed net density;

L A tree survey prepared in conformance with [Section 19.30.060](#) and meeting the tree protection requirements of [Section 19.30.080](#);

M. The names and addresses of all persons having an ownership interest in the property or in the applicant. If the property is owned by, or the applicant is a partnership, limited liability company, corporation or other legal entity, then the names and addresses of all persons having a financial interest in the legal entity or entities shall be provided;”

Given the above missing information, the application should be returned to the applicant as incomplete. The public should be provided a new notice of application if the applicant later submits with the above and any other missing materials.

**3. The Preliminary Plat fails to meet the following subdivision approval criteria:**

“BDMC 17.15.020.A.

3. The public use and interest is served by the establishment of the subdivision and dedication. In considering this criteria, it shall be determined if appropriate provisions are made for all relevant matters, including, but not limited to, the public health, safety and general welfare,

4. The physical characteristics of the proposed subdivision site, as conditioned, do not increase the risk of flood or inundation conditions on- or off-site;

5. Applicable city development standards are met or exceeded;

6. All environmental impacts have been addressed consistent with the public health, safety and welfare and city goals and policies;

7. Concurrency exists for all utilities and transportation system improvements prior to occupancy of any structures;“

**4. The Wetland category and buffer data is not accurate and not based on current information.**

It has been six years since the wetland sensitive area category review. Guidance based on wetland science and adopted by the state Department of Ecology and federal Army Corps of engineers indicates that a wetland delineation and rating may only be relied upon for five years. Thus, new field work is necessary.



According to the *Plat 2F Sensitive Area Study*: “On May 11, 2017, WRI evaluated the categories and buffers of Wetlands P/Q, ZZ, 215, Y, 105, 214, and 216”. (page 2).

5. Observed water levels in wetlands on and near the plat (especially the core wetland complex) have been higher for several years now than what they were at the time the 2007 field work was done. Based on this new information, a new wetland delineation is required under SEPA.
6. The Plat 2F Buffer Averaging proposal does not meet requirements to protect and improve wetland function and violates requirement that a buffer averaging buffer reduction not reduce a buffer by more than 50% at any location.

The *Plat 2F Sensitive Area Study* makes an unsupported statement without providing evidence on how the requirement will be met that, “Equivalent functions will be provided through buffer averaging by meeting or exceeding the guidelines established in BDMC §19.10.230(H) and the Ecology guidance in Wetlands in Washington State, Volume 2, Guidance for Protecting and Managing Wetlands.” (page 7, section 1.3 on buffer averaging)

The Sensitive Area Study incorrectly states that there is no other feasible option for roads, citing an exception allowed by BDMC 19.10.220(C)(4). (page 5). This exception is not applicable because it is not true that there is no other feasible option. The developer would simply need to rearrange and possibly reduce the number of housing units in the plat. This is completely feasible and thus portions of their buffer reduction are illegal.

7. The requested deviations for the sidewalk and vegetative separator violate the letter and spirit of the road and pedestrian requirements.

The developer can fit an appropriately sized sidewalk or path complete **with** the vegetated separator from vehicles, but they have chosen not to. They are requesting deviations and claiming that meeting MPD and city standards is not feasible. However, it is feasible if the plat layout is re-arranged and, if necessary, some of the housing lots removed. Land area used for transportation, sensitive areas, and buffers is removed from the land area total before calculating net density and thus before determining if the plan meets allowed / required density.

The requested deviations also violate the applicable MPD design requirements.



## SECTION: Ten Trails Development Agreement Ordinance 970 violations.

### (1) Late satellite fire station: no construction contract by 750th unit. Not under construction 1.5 years later at over 1,000 occupied units.

My name is Gary Davis. I live in the Rainier View development over here on 2<sup>nd</sup> Ave. Last winter, I was taking my recycling out during the ice storm because I didn't realize everything had turned into an ice-skating rink.

Of course, I fell, and with all the ice I could not get back up. I'm lucky because a neighbor came over to check on me and called 911. I was not so lucky because the response from MountainView fire took, I would say, at least 30 minutes. A fire station staffed on this side of town would have allowed a shorter response, even with the weather and potential other more urgent calls at the same time.

I think we build our Emergency Response service with the expectation there will be more calls when there is some extreme event, and we prepare for it. That's why we set a "Level of Service" for the fire department, and why Black Diamond knows we need to build another fire station as population grows. This is part of Growth Management.

Of course, this was known in 2010. The Ten Trails Development Agreement requires that the developer build two fire stations. **The first is overdue. City Ordinance 970 section 13.1.4.iii subsection "b" says: "...the construction contract shall be awarded no later than the time of issuance of a Certificate of Occupancy for the 750th Dwelling Unit."**

The printed handouts we brought contain that section from the Development Agreement, as well as a city report showing there were 775 Ten Trails units occupied by the end of 2021. The construction contract is now a year and a half late. Since then, the developer has been allowed to add more and more units bringing us to at least 1000 to occupied by the end of last year. And none of us have adequate fire protection.

The longer it takes to get that fire station in place while we continue to allow our population to increase means that inevitably some sick person will be forced to wait too long, and their health will be harmed.

The handouts also show a public records request for any contract for the fire station. The city response confirms was no construction contract. **What's also disturbing is, at two separate council meetings (June 16 and August 11, 2022) the Director incorrectly stated there is a fire station contract to meet this requirement.**

Further, the requirement is not really met if a construction contract just sits on the shelf. Until the developer comes into compliance, the city should not accept any further subdivision applications. How can more development governed by the Development Agreement be valid while the DA is being violated? Thank you.



(1) Late satellite fire station: no construction contract by 750th unit. Not under construction 1.5 years later at over 1,000 occupied units.

*Ord. 970, Development Agreement, Fire Mitigation Section 13.4.D.iii.(b), pg. 123:*

**The Villages Master Planned Development  
Development Agreement**

Developer, then the Master Developer shall provide and/or acquire a site for the satellite fire station as credit against existing or future fire mitigation or impact fees.

- iii. Construction. The Master Developer shall construct or cause to construct, the satellite fire station designed pursuant to subsection D(i) above on the site selected pursuant to subsection D(ii) above as credit against existing or future fire mitigation or impact fees. Master Developer shall cause the letting of a contract for construction of such fire station as follows:
  - a. If the construction of the satellite fire station is financed pursuant to a Community Facilities District (CFD) established under RCW Ch. 36.145, the construction contract shall be awarded no later than the time of issuance of a Certificate of Occupancy for the 500th Dwelling Unit; or
  - b. If the construction of the satellite fire station is not financed pursuant to a Community Facilities District established under RCW Ch. 36.145, the construction contract shall be awarded no later than the time of issuance of a Certificate of Occupancy for the 750th Dwelling Unit.
- iv. Equipment. The Master Developer shall provide the fire and/or emergency response apparatus reasonably necessary for operation of the satellite fire station described in this subsection D prior to such station commencing operation as credit against existing or future fire mitigation or impact fees subject to the following conditions:
  - a. Pursuant to the Fire Impact Fee Study, apparatus shall not exceed the following: an engine (\$726,856), aid car (\$251,420), staff vehicle (\$55,000), and brush truck (\$90,000).
- v. Funding. The design, site selection, construction, and equipment described in this subsection, shall be funded as follows:



(1) Late satellite fire station: no construction contract by 750th unit. Not under construction 1.5 years later at over 1,000 occupied units.

City of Black Diamond Report showing 775 occupied dwelling units by **2021**:

Copy of New Home Sales at Ten Trails.xlsx [Protected View]					
	A	B	C	D	E
1	<b>City of Black Diamond</b>				
2	<b>Master Planned Development</b>				
3	<b>Ten Trails Homes Sales/Closings</b> (Based on new City water accounts)				
4	<b>As of December 31, 2021</b>				
5					
6	<b>MONTH</b>	<b>2019</b>	<b>2020</b>	<b>2021</b>	<b>2022</b>
7	January	7	16	13	22
8	February	8	27	13	
9	March	13	24	27	
10	April	9	12	21	
11	May	12	19	20	
12	June	9	21	29	
13	July	18	24	9	
14	August	12	23	16	
15	September	7	19	19	
16	October	13	23	19	
17	November	19	25	23	
18	December	16	16	18	
19	<b>TOTAL</b>	<b>143</b>	<b>249</b>	<b>227</b>	<b>22</b>
20	<b>*11 Homes were sold in 2018*</b>				<b>* 11</b>
21	<b>** 76 apartment units became occupied in 2021**</b>				<b>** 123</b>
22	<b>** 47 Rental homes by AMH Drv LLC V24 are occupied**</b>				<b>TOTAL 775</b>
23					
24	2018 2nd half	11	11		
25	2019 1st half	58	69		
26	2019 2nd half	85	154		
27	2020 1st half	119	273		
28	2020 2nd half	130	403		
29	2021 1st half	123	526		
30	2021 2nd half	104	630		
31	2022 1st half	22	652		
32	2022 2nd half	0	652		
33	<b>Running Total</b>		<b>652</b>		



(1) Late satellite fire station: no construction contract by 750th unit. Not under construction 1.5 years later at over 1,000 occupied units.

Public Records Request for fire station construction contract:

**PRR Bowie 22-070\_Construction Contract (fire dept)**

Carina Thornquist

To: gbowie2014@gmail.com

CC: Brenda Martinez

Sent: 8/30/2022 4:45 PM

Dear Mr. Bowie,

Thank you for your records request which we received on 8/25 regarding Construction Contract (fire dept).

I double checked to see if there were any additional contracts that Andy spoke of at the joint meeting. The result was only the one page document.

The requested documents have been assembled on a thumb drive (along with PRR 22-068) and are waiting at the front desk for you. The cost for a thumb drive is \$2.96. I would suggest paying by either cash or check as they charge \$3.00 to process a credit card.

The City will now consider this request closed.

Please don't hesitate to reach out to me should you need any further assistance.

Regards,

**Carina A. Thornquist**

Deputy City Clerk

City of Black Diamond



(1) Late satellite fire station: no construction contract by 750th unit. Not under construction 1.5 years later at over 1,000 occupied units.

Only document provided in response to above records request does not include construction, just "clearing, grading, striping, grubbing, and erosion control" :

PLN22-0018  
Fire Station

**FILE COPY**

**OAKPOINTE**

April 5, 2022

City of Black Diamond  
Andy Williamson, MDRT Director  
24301 Roberts Drive  
Black Diamond, WA 98010

**RE: Lawson Hills Fire Station Contractor**

Dear Mr. Williamson,

This letter is to inform you that CCD Black Diamond Partners LLC has entered into a General Contractor Agreement with Goodfellow Bros. LLC to provide labor, equipment, and materials necessary for clearing, grading, striping, grubbing and erosion control on King County Parcel No. 1321069022, also known as the Lawson Hills Fire Station.

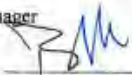
Goodfellow Bros. LLC will begin work upon written notification by Oakpointe to proceed.

If you have any questions or would like to discuss further, please let us know.

Owner:

CCD Black Diamond Partners LLC, a Delaware limited liability company

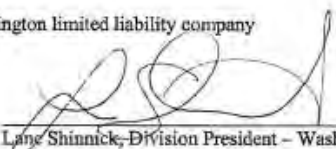
By: Oakpointe LLC, its Manager

By:   
Brian Ross, Manager

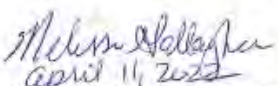
Date: 4.11.22

General Contractor:

Goodfellow Bros. LLC, a Washington limited liability company

By:   
Lane Shinnick, Division President - Washington

Date: 4-11-22

  
April 11, 2022

MELISSA GALLAGHER  
Notary Public  
State of Washington  
Commission # 164459  
My Comm. Expires Feb 26, 2025

3025 112<sup>th</sup> Ave NE SUITE 100  
BELLEVUE, WA 98004  
WWW.OAKPOINTE.COM



## (2) City athletic fields not paid for or constructed by 800 units.

Cities in the state of Washington are required to have Parks and Recreation standards. So, in 2008, Black Diamond adopted a fairly typical Parks Plan. One of the things this plan did was set the number of sports fields needed based on population size.

This was important because the city was then able to require a large developer seeking a 15-year development agreement, to pay for sports fields. The fields are for the whole city, not just Ten Trails.

Public input during the environmental review and development agreement hearing process was also extremely important. The developer proposed paying a set fee per house, but the public said a set fee would be insufficient. Therefore, Section 9.5.3.B of the Development Agreement requires the developer either build the fields, or that public bid be advertised to construct the fields when Ten Trails reaches 800 units. The developer must pay the actual cost. This was good for government finances and taxpayers. But what good is it if it's not followed?

Ordinance 970 , the Development Agreement, sets the first sports field requirements. Table 9.5.5 requires a soccer field, a baseball field, and a tennis court by the 800th dwelling unit. In the past couple of months at public comment to the City Council, it was pointed out twice that the development now has over 1,000 units, but the city has not gotten the sports fields or the payment.

Then, City Council heard two Ten Trails Development updates over the last month: one from the City staff and one from the developer, OakPointe. *Those presentations didn't mention the sports fields requirement nor the status of any of the requirements in this report.* We haven't been given an update on when the city will collect the money or an update on any public bid, payment, or construction plans.

Some people have wondered if the fields were supposed to be built as part of the proposed elementary school. *No.* See Development Agreement section 9.5.2. There has been no agreement to provide these fields on the school site. Even if such an agreement were made it cannot change the timing threshold of 800 units.

**While the developer is in violation of the Development Agreement Ordinance, the city should stop accepting new subdivision applications.**



## (2) City-wide Athletic Fields Not Funded or Constructed by 800 units as Required.

The following pages of the handout contain the parts of Ordinance 970, Development Agreement section 9.5 referenced above with the sports fields requirements.

*Ord. 970, Development Agreement, Section 9.5, pg. 79:*

**9.5.2** Unless otherwise noted on Table 9-5, Recreational Facilities constructed by the Master Developer, may be located: (1) within The Villages MPD in Community Parks, community center or Neighborhood Parks; (2) on joint use school sites (if agreed to by the City and School District as provided in the School Agreement); (3) within off-site Regional Parks (subject to City agreement); or (4) on a mutually acceptable off-site location. The Recreational Facilities may be provided in combination with one another and other informal space or each facility may be provided as a standalone amenity. Pursuant to Condition of Approval No. 88 of the MPD Permit Approval, if a joint use facility is proposed on a school site or on an alternative site consistent with the School Agreement, the Master Developer shall provide for one or more youth/adult softball fields, soccer fields, tennis courts or basketball courts in such joint use facility.

**9.5.3** The Master Developer shall have the option to request that the Designated Official accept a lump sum payment in lieu of constructing any of the individual Recreational Facilities in Table 9-5. The request shall be made prior to triggering the need for the next Recreation Facility. Pursuant to Condition of Approval No. 93 of the MPD Permit Approval, the Designated Official retains sole discretion to determine when and if a lump sum payment will be accepted in lieu of the Master Developer constructing a Recreational Facility. The Designated Official's determination shall be based on the following three criteria: (i) availability of land; (ii) adequacy of funds to construct City-approved recreational facilities; and (iii) City's ability to maintain recreational facilities. Pursuant to Condition of Approval No. 91 of the MPD Permit Approval, the amount of the payment that may be provided in lieu of construction shall be set through the following process:

- A. Commencing upon the Designated Official's agreement to accept a lump sum amount for a specific Recreational Facility, the City shall publish a bid request for design and construction of the Recreational Facility. The bid request shall be based on reasonable standards agreed to by the Master Developer and City. The City reserves the right to include additional elements in the bid beyond what the Master Developer is required to construct for the facility. Such additional elements shall not be the responsibility for the Master Developer to fund. For instance, additional elements may include lighting, concession areas, or other elements as determined by the Designated Official.



(2) City-wide Athletic Fields Not Funded or Constructed by 800 units as Required.

Ord. 970, Development Agreement, Section 9.5, Pg. 80:

The Villages Master Planned Development Development Agreement	
B. Upon acceptance of a bid, the Designated Official and Master Developer shall agree to set the lump sum amount in accordance with the accepted bid. Upon execution of a contract accepting the bid for design and construction, the Master Developer shall <u>deposit with the City the agreed to lump sum amount.</u>	

Ord. 970, Development Agreement, Section 9.5, pgs. 81-82:

The Villages Master Planned Development Development Agreement				
9.5.5 Recreation Facilities				
TABLE 9-5				
Facility Type	Level of Service	Required Facilities	Timing of Facilities	Fee-in-Lieu
Basketball Court	1:2000	6	Master Developer shall provide a minimum of one (1) Basketball Court facility per every 800 Dwelling Units constructed. One such facility must be Constructed (or, if applicable, a fee-in-lieu paid) prior to the 800 <sup>th</sup> , 1600 <sup>th</sup> , 2400 <sup>th</sup> , 3000 <sup>th</sup> , 3600 <sup>th</sup> , and 4800 <sup>th</sup> Unit receiving a Certificate of Occupancy.	The Master Developer may elect to request that the City accept a fee-in lieu of constructing the required Basketball Courts. The fee shall be set per Section 9.5.3.
Soccer Field	1:2000	6	Master Developer shall provide a minimum of one (1) soccer field per every 800 Dwelling Units constructed. One soccer field must be Constructed (or, if applicable, a fee-in-lieu paid) prior to the 800 <sup>th</sup> , 1600 <sup>th</sup> , 2400 <sup>th</sup> , 3000 <sup>th</sup> , 3600 <sup>th</sup> , and 4800 <sup>th</sup> Unit receiving a Certificate of Occupancy. Up to three (3) of the required soccer fields shall be designed as Micro Soccer Fields.	The Master Developer may elect to request that the City accept a fee-in lieu of constructing the required soccer fields. The fee shall be set per Section 9.5.3.



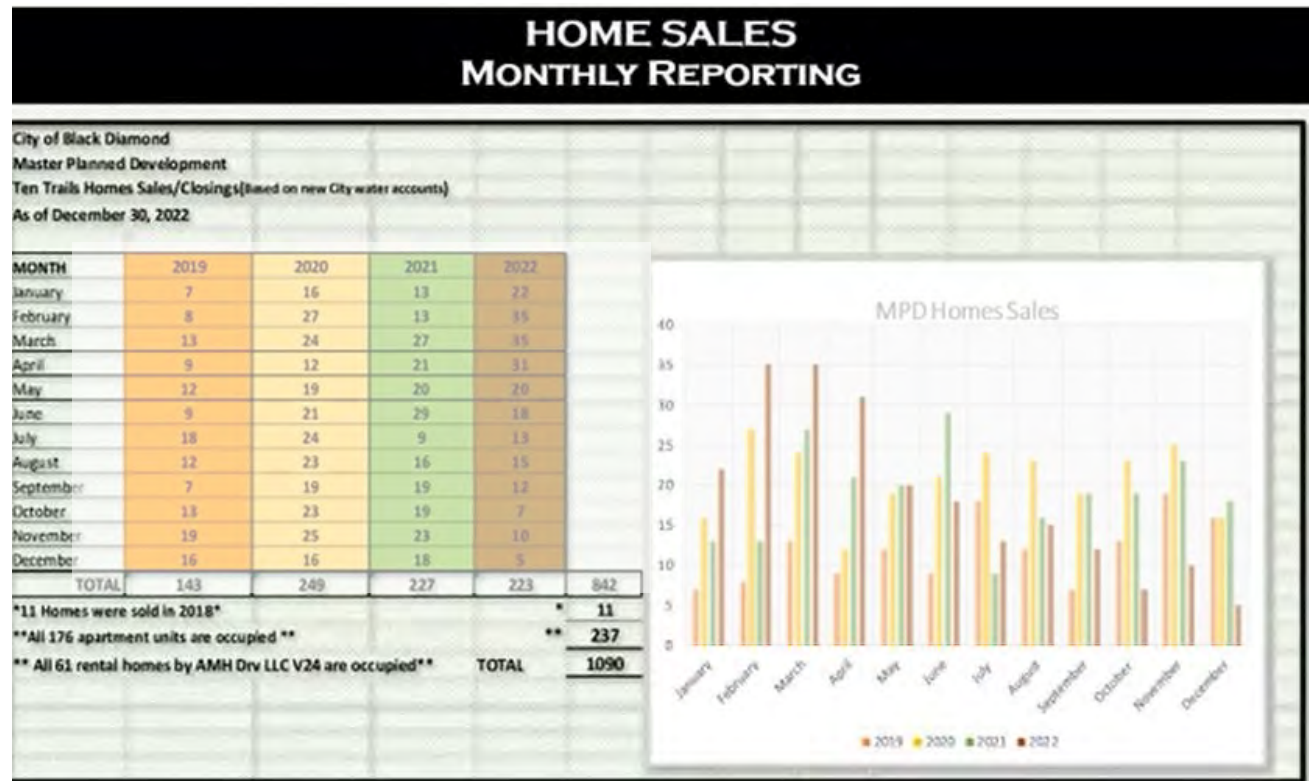
(2) City-wide Athletic Fields Not Funded or Constructed by 800 units as Required.

The Villages Master Planned Development Development Agreement				
Facility Type	Level of Service	Required Facilities	Timing of Facilities	Fee-in-Lieu
Tennis Court	1:2000	6	Master Developer shall provide a minimum of one (1) tennis court per every 800 Dwelling Units constructed. One tennis court must be Constructed (or, if applicable, a fee-in-lieu paid) prior to the 800 <sup>th</sup> , 1600 <sup>th</sup> , 2400 <sup>th</sup> , 3000 <sup>th</sup> , 3600 <sup>th</sup> , and 4800 <sup>th</sup> Unit receiving a Certificate of Occupancy.	The Master Developer may elect to request that the City accept a fee-in lieu of construction for up to 3 of the tennis courts. The fee shall be set per Section 9.5.3.
Youth Baseball/Adult Softball field	1:2000	6	Master Developer shall provide a minimum of one (1) Youth Baseball / Adult Softball field per every 800 Dwelling Units constructed. One such field must be Constructed (or, if applicable, a fee-in-lieu paid) prior to the 800 <sup>th</sup> , 1600 <sup>th</sup> , 2400 <sup>th</sup> , 3000 <sup>th</sup> , 3600 <sup>th</sup> , and 4800 <sup>th</sup> Unit receiving a Certificate of Occupancy.	The Master Developer may elect to request that the City accept a fee in lieu of constructing the necessary facility. The fee shall be set per Section 9.5.3.



## (2) City-wide Athletic Fields Not Funded or Constructed by 800 units as Required.

City staff presentation March 2, 2023, showing over 1,090 occupied units by end of 2022:





### (3) Failure to meet fiscal requirement that development revenue provide sufficient funding for Fire and Police staff to serve growth.

Ordinance 970, the Ten Trails Development Agreement, requires that the increase in fire and police operational costs due to new growth be covered by new tax revenue brought in from the development. If the new tax revenue is insufficient, the Ordinance has fail-safe provisions that require developer OakPointe to make up the difference.

Specifically, Section 13.6.1 requires that a “fiscal analysis” be completed every five years to determine if new tax revenue is sufficient to maintain levels of service for police and fire. Section 13.6.6 then requires that every year, an “annual review” compare the projections of that fiscal analysis to the budget.

However, if you listen to the recent City meetings on the budget and on fire and police operational costs, you would never know that these requirements exist.

Council had a meeting on April 27, 2023, where the same consultant who reviewed the developers’ most recent Fiscal Analysis presented a separate city budget forecast out to the year 2030. The Consultant recommended increasing taxes to pay for fire and police staff increases.

What happened? Why didn't staff or the fiscal consultant describe Ordinance 970 Section 13.6.1's requirement that new development pay for increased costs for fire and police?

Without any discussion or public review of how much new Development-related revenue needed to be set aside for police and fire operational cost increases, it appears that new revenue was spent on other things.

Now, within only a few months of a slowdown in home sales, the city council is paying a consultant to recommend raising taxes. **Instead, I ask the Council to discuss and make public all documentation on why the police and fire operational increases are not supported by new development as required by Ordinance 970.**

I also ask the Council to reduce spending. Change the current budget, decline expenditures if they aren't as important as new fire and police staff.

We have a problem providing public safety for the people that are already here, so why are we adding to the problem by approving more development? The City should **not accept more applications for subdivisions governed by the Development Agreement while the Development Agreement is being violated.**



(3) Failure to meet fiscal requirement that development revenue provide sufficient funding for Fire and Police staff to serve growth.

Legal Requirements and Level of Services related to Police and Fire Operations:

Black Diamond Municipal Code (BDMC) 18.98.080(A)(5):

“The project, at all phases and at build-out, will not result in the lowering of established staffing levels of service including those related to public safety.”

*Ord 946, Approval of Master Planned Development, Ex B Conclusions of Law, pg. 23:*

“A condition of approval {No. 100} has been added to Exhibit C to require that the Development Agreement include specific provisions for mitigating fire service impacts to ensure protection concurrent with project build out. The conditions of approval regarding fiscal impacts also include a condition (No. 156) that requires that the fiscal analysis ensure that revenues from the project are sufficient to pay the project's pro rata share to maintain staffing levels of service.”

*Ord. 970, Development Agreement Section 13.6(1)(i), pg. 129:*

“... Each updated fiscal analysis shall confirm that revenue from The Villages MPD is sufficient to maintain levels of service for police and fire services as such levels of service are adopted in the Comprehensive Plan (Exhibit ‘E’).”

*Vested City of Black Diamond Comprehensive Plan June 2009, pg. 8-9:*

**8.4.3. Level of Service**

The current LOS for police is 3.5 officers per 1,000 residents. As the City grows, the LOS standard can be reduced to 2.75 officers per 1,000 residents through efficiency gains in the provision of police services. The LOS for police is proposed to decrease with each 1,500 to 2,000 increment of population growth, as shown in Table 8-1. .

Table 8-1. Police Level of Service

Population Level	4,000-5,000	5,000-7,500	7,500-10,000	10,000-13,000	13,000-16,000	16,000-20,000
Police Officers	8	8.2	12.5	14.7	21	29
Sergeants	2.6	2.3	3.25	3.7	4.6	6.25
Administration	1.5	1.6	1.6	2	2.6	4.75
Total Staff	12.1	12.1	17.35	20.4	28.2	40

*Vested City of Black Diamond Comprehensive Plan June 2009, pg. 8-24:*

**8.7.3. Level of Service**

The City has an LOS standard of 1.4 on-duty career firefighters per 1,000 population.



(3) Failure to meet fiscal requirement that development revenue provide sufficient funding for Fire and Police staff to serve growth.

*Ord. 970, Development Agreement Section 13.6.5(a) and .6, pg. 133:*

“13.6.5(a)

.... Possible options for addressing the shortfall may include, but are not limited to:

... ii. Pursuant to Condition of Approval No. 156, interim funding of necessary service and maintenance costs (staff and equipment) between the time of individual project entitlements and off-setting tax revenues. However, if a deficit is projected as part of the fiscal analysis for Phase 3, then a payment shall not be accepted by the City.

6. Annual review of Fiscal Results

a. As part of the Annual Review pursuant to the terms of the Funding Agreement, the Designated Official and Master Developer shall meet to review the projections of the Fiscal Analysis compared to the City's budget.

i. If interim funding is provided pursuant to subsection 5.a.ii above, then the Annual Review shall include development of a payment schedule. The payment schedule shall be determined by comparing the projected revenues and expenses shown in the fiscal analysis to the City's projected budget for the upcoming calendar year.”

*Ord. 946 The Villages MPD, Ex. C - Conditions of Approval, Condition 156.b, pg. 27 of 29:*

“156. ... The applicant shall be responsible for addressing any projected city fiscal shortfall that is identified in the fiscal projections required by this condition. This shall include provisions for interim funding of necessary service and maintenance costs (staff and equipment) between the time of individual project entitlements and off-setting tax revenues; provided, however, that in the event that the fiscal projection prepared prior to the commencement of Phase III indicates a likelihood of significant ongoing deficits in the city's general fund associated with operations or maintenance for properties within the MPD, the applicant must address the projected shortfalls by means other than interim funding.”



(3) Failure to meet fiscal requirement that development revenue provide sufficient funding for Fire and Police staff to serve growth.

For informational purposes because the Fiscal Analysis is related to the Phasing plan, *Ord 970, Development Agreement, Exhibit K Phasing Plan*, pg. 9-1 thru 9-8:

“The phasing plan includes 4 phases: 1A, 1B, 2, and 3....

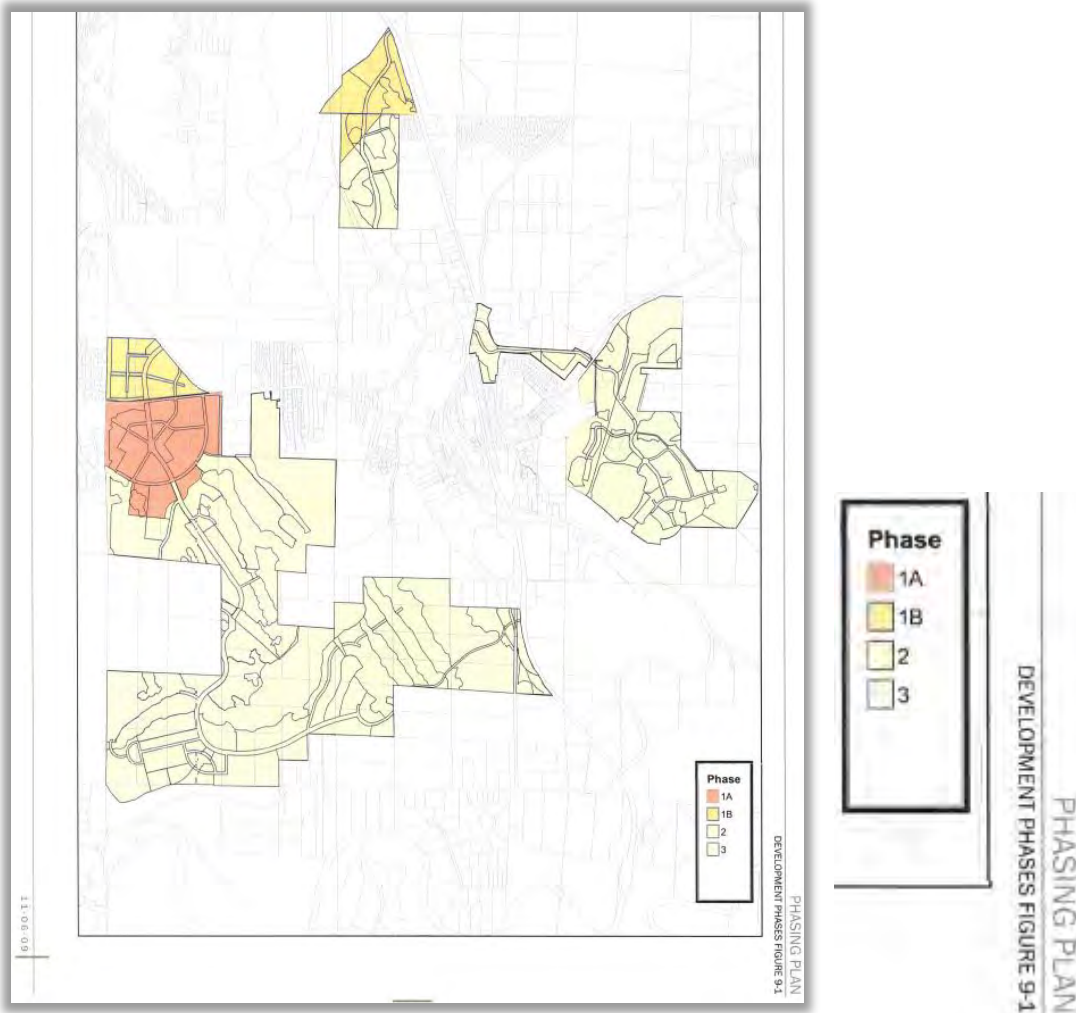
Phase 1A includes approximately 130 acres containing approximately 850 dwelling units ...

Phase 1B includes approximately 120 acres, 66 within the villages and 54 within Lawson hills and approximately 200 dwelling units...

Phase 2 consists of approximately 394 acres, 73 acres in the Lawson hills MPD and 321 acres in the villages MPD, with approximately 1500 total dwelling units. ...

Phase 3 consists of approximately 926 acres, 247 acres in the Lawson Hills MPD and 679 acres in the villages MPD, approximately 3500 total dwelling units.”

*Ord 970, Development Agreement, Exhibit K Phasing Plan Figure 9-1:*





#### (4) Failure to complete Master Planned Development (MPD) Fiscal Analysis every 5 years and ensure the MPD does not have adverse financial impact.

The Black Diamond Municipal Code and OakPointe Development Agreements require that the Master Planned Development (MPD or Ten Trails) not have an adverse financial impact on the city. To ensure this, a “Fiscal Analysis” must be updated for the MPD every five years and at the beginning of every phase. (BDMC 18.98.080.A.3 and Ord. 946 condition 156.a).

The Fiscal Analysis is a forecast of revenue and costs associated with MPD growth. City revenue from new development includes things like the city’s share of annual property tax, one-time taxes such as when real estate is sold, and ongoing taxes such as sales tax from spending by new residents at both existing and new Black Diamond businesses. (For further detail see: Ordinance 970 exhibit N - Funding Agreement, and the latest fiscal analysis, available by doing a public records request to your city.)

The first OakPointe MPD Fiscal Analysis was accepted by the City in May 2014, so the next was due in 2019. The City Council approved a contract in 2020 for a financial consultant to review a draft Fiscal Analysis, but the analysis and acceptance were not complete until 2021. That means for two years, the developer was allowed to continue building while in violation of this development agreement requirement.

Why does violation of the requirement to have a Fiscal Analysis every 5 years matter? Here is why: Letting the analysis be two years late means that now, instead of getting a new analysis in 2024 (5 years after 2019), the city won’t get one until 2026.

Unfortunately, the analysis completed in 2021 was almost immediately outdated. It forecast sales tax for 2022 from the still-empty Ten Trails retail area.

If we subtract this revenue we are not getting, is there now an adverse financial impact on the city?

Development Agreement Section 13.6 and Ord 945 Condition 156 require the developer to address any shortfall, which may include, “interim funding of necessary service and maintenance costs (staff and equipment) between the time of individual project entitlements and off-setting tax revenues.” If we don’t enforce this condition, then how will we pay for the new services to serve our growing population?

Thank you.



**(4) Failure to complete Master Planned Development (MPD) Fiscal Analysis every 5 years and ensure the MPD does not have adverse financial impact.**

MPD Fiscal Analysis Requirements:

*Black Diamond Municipal Code (BDMC) 18.98.080 - MPD permit—Conditions of Approval.*

"A. An MPD permit shall not be approved unless it is found to meet the intent of the following criteria or that appropriate conditions are imposed so that the objectives of the criteria are met: ...

3. The proposed project will have no adverse financial impact upon the city at each phase of development, as well as at full build-out. The fiscal analysis shall also include the operation and maintenance costs to the city for operating, maintaining and replacing public facilities required to be constructed as a condition of MPD approval or any implementing approvals related thereto. This shall include conditioning any approval so that the fiscal analysis is updated to show continued compliance with this criteria, in accordance with the following schedule:

- a. If any phase has not been completed within five years, a new fiscal analysis must be completed with regards to that phase before an extension can be granted; and
- b. Prior to commencing a new phase."

*Ord. 946 The Villages MPD, Ex. C - Conditions of Approval, Condition 156, pg. 27 of 29:*

"156. The proposed project shall have no adverse financial impact upon the city, as determined after each phase of development and at full build-out. The required fiscal analysis shall include the costs to the city for operating, maintaining and replacing public facilities required to be constructed as a condition of MPD approval or any implementing approvals related thereto. The fiscal analysis shall ensure that revenues from the project are sufficient to maintain the project's proportionate share of adopted City staffing levels of service. The fiscal analysis shall be updated to show continued compliance with this criterion, in accordance with the following schedule:

- a. Within five years, a new fiscal analysis shall be completed to determine the long- term fiscal impact to the City. If necessary, additional project conditions may be required.
- b. Prior to commencing a new phase, including the first phase of construction. The exact terms and process for performing the fiscal analysis and evaluating fiscal impacts shall be outlined in the Development Agreement, and shall include a specific "MPD Funding Agreement," which shall replace the existing City of Black Diamond Staff and Facilities Funding Agreement. The applicant shall be responsible for addressing any projected city fiscal shortfall that is identified in the fiscal projections required by this condition. This shall include provisions for interim funding of necessary service and maintenance costs (staff and equipment) between the time of individual project entitlements and off-setting tax revenues; provided, however, that in the event that the fiscal

**(4) Failure to complete Master Planned Development (MPD) Fiscal Analysis every 5 years and ensure the MPD does not have adverse financial impact.**

projection prepared prior to the commencement of Phase III indicates a likelihood of significant ongoing deficits in the city's general fund associated with operations or maintenance for properties within the MPD, the applicant must address the projected shortfalls by means other than interim funding.”



## (5) Fixes for failing SR 169 intersections at Ravensdale Rd. and Roberts Dr. not yet permitted, but City allowing further Ten Trails occupancy in violation of Development Agreement section 11.4.

According to our city's Comprehensive Plan, our state Growth Management Act, and our State Environmental Policy Act, traffic is far from the only consideration when approving and planning for development.

Transportation congestion is a problem that affects our daily lives, so it gets a lot of attention. City Ordinance 970, the Ten Trails Development Agreement, contains strong language to protect the public from unsafe and unreasonable delays at intersections.

Ord. 970 Section 11.4 (below) and its "Traffic Monitoring Plan," required a "Regional Infrastructure Improvements" schedule be created prior to the first phase of Ten Trails development. This schedule set the trigger of at 327 Ten Trails units for construction of Highway 169 fixes for Ravensdale Road and Roberts Drive.

The schedule also was created with awareness of the need to get permit materials early to the State Department of Transportation, or WashDOT. It said that complete "engineering, design and construction drawings, and related application materials necessary for permit issuance" must be submitted to WashDOT prior to the city's issuance of the first residential building permit in Ten Trails. The developer's compliance was questionable: they first submitted a traffic signal design, not the preferred roundabout, and the application was incomplete.

The schedule also requires:

"The Master Developer shall diligently pursue issuance of all permits for this interim improvement, by taking such actions including but not limited to promptly responding to requests for additional information and promptly submitting permit application revisions requested by the permitting agencies."

*But wait, there's more.* Ordinance 970 section 11.4 requires that occupancy of new houses is not allowed until after the "regional facility" has been permitted. The regional facilities in this case are the failing intersections on State Route 169. (Those are listed in the Table 11-5-1 as a master developer funding responsibility.)

The Traffic Monitoring Plan has additional fine print that allows occupancy beyond 327 units *only if* reasonably necessary due to events outside of the master developer's control. However, the master developer was slow to answer WSDOT's requests time and time again making the process take years longer than necessary. I have done regular records requests that prove OakPointe did not respond promptly to WSDOT and I can provide those records if you email me. Because these events were not outside the master developer's control, the 327-unit occupancy maximum should still apply.

## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

When you drive through the city on highway 169, remember we're talking about delays at the side stop signs that can be up to 15 minutes, leading some drivers to make dangerous decisions. I have two kids and we have to use this intersection daily to get to and from work and school. Developer OakPointe now has over 1,000 occupied units ... If 327 units was the time construction was supposed to start, at what point might we finally decide this is unreasonable?

We've been told the fix for the intersections will take several years of construction. Road construction for the water line project currently on highway 169 causes this intersection to back up for 15 minutes most days... and we would consider allowing further occupancy of the master developer's project before and during this construction??

The City should **not accept more applications for subdivisions governed by the Development Agreement while the Development Agreement is being violated.**

*Ord. 970, Development Agreement, Section 11.4, pgs. 99 - 106:*

"11.4 PHASING AND CONSTRUCTION OF OFF-SITE REGIONAL INFRASTRUCTURE  
...to serve The [OakPointe Ten Trails] Villages MPD during its initial Phases, construction is tied to thresholds .... as to transportation, documents that result of the Traffic Monitoring Plan (Exhibit "F"), shall be submitted to the Designated Official for approval.

**Occupancy of an Implementing Project that exceeds the construction threshold is allowed after the necessary Regional Facility has been permitted. ...**

B. Construction and Funding. ... the Master Developer shall design and Construct (or cause to be Constructed) the off- site Regional Facilities identified ... below. ...

Table 11-5-1. Transportation Intersection Improvements

Study Intersection	Funding Responsibility
... SR 169/SE 288th Street	Master Developer
... SR 169/Roberts Drive	Master Developer
... SR 169/SE Black Diamond Ravensdale Road (Pipeline Road)	Master Developer

Table 11-5-2. Transportation Roadway Improvements

... Construct portion of North Connector from SR-169 south to boundary of Phase IB area  
... Continue Construction of North Connector from end of Phase IB construction to Pipeline Road  
... Construct Pipeline Road from SR-169 to Lake Sawyer Rd SE"

*August 25, 2012, Phase 1A Regional Infrastructure Improvements, page 6-7:*

**"SR 169/ Roberts Drive** (Interim Improvement)



## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

Completed engineering, design and construction drawings and related application materials necessary for permit issuance for this interim improvement will be submitted to WSDOT prior to the City of Black Diamond's issuance of the first residential or commercial building permit associated with Division 1A of Preliminary Plat I A. The Master Developer shall diligently pursue issuance of all permits for this interim improvement, by taking such actions including but not limited to promptly responding to requests for additional information and/or promptly submitting permit application revisions requested by the permitting agencies. Construction of this improvement will commence as soon as reasonably practicable following issuance of all necessary permits, but in any event **prior to issuance of the building permit for the 327th Equivalent Residential Unit (ERU)** within Preliminary Plat 1A plus any additional time demonstrated to the reasonable satisfaction of the City's Designated Official to be necessary due to action, inaction, or events outside of the Master Developer's control.

### Interim Improvement Details

This intersection will be improved by shifting Roberts Drive to the south to provide approximately 500 feet separation between it and SE Black Diamond - Ravensdale Road. The SR169 / Roberts Drive intersection will be reconfigured to form a tee intersection instead of the currently existing Y configuration to improve safety and operation. This intersection improvement will include installation of a signal to control the intersection. Intersection improvements will include construction of a right turn lane on southbound SR 169. Roberts Drive behind the stop bar location at SR 169 will be designed per City of Black Diamond standards. The ultimate design is a roundabout - this is only an interim improvement.

### SR 169/SE Black Diamond - Ravensdale Road (Interim Improvement)

Completed engineering, design and construction drawings and related application materials necessary for permit issuance for this interim improvement will be submitted to WSDOT prior to the City of Black Diamond's issuance of the first residential or commercial building permit associated with Division 1A of Preliminary Plat I A. The Master Developer shall diligently pursue issuance of all permits for this interim improvement, by taking such actions including but not limited to promptly responding to requests for additional information and/or promptly submitting permit application revisions requested by the permitting agencies. Construction of this improvement will commence as soon as reasonably practicable following issuance of all necessary permits, but in any event **prior to issuance of the building permit for the 327th Equivalent Residential Unit (ERU)** within Preliminary Plat 1A plus any additional time demonstrated to the reasonable satisfaction of the City's Designated Official to be necessary due to action, inaction, or events outside of the Master Developer's control.

### Interim Improvement Details

This intersection improvement includes **a four way signalized intersection** to maintain access to the Palmer Coking Coal property at this location. Black Diamond- Ravensdale Road SE behind the stop bar location at SR 169 will be designed per City of Black Diamond standards. The ultimate design is a roundabout - this is only an interim improvement."

(5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

(6) Failure to enforce Affordable Housing “balanced share” commitment in OakPointe Master Planned Developments (MPDs)

1. Our region has housing affordability needs. We empathize with the large number of people whose budget doesn’t stretch to afford decent housing.

Black Diamond is in the process of adding quadruple its previous 1,500 households with 6,000 dwelling units approved for developer OakPointe. Additional housing has been built or proposed in the city recently. Black Diamond is doing more than its share.

2. Affordable Housing requirement for the MPD is not being enforced.

OakPointe is required to provide a balanced share of affordable housing in Ten Trails. This means 20% of the units should be affordable for people making far less than the “area median income.” This was a condition of enlarging the city limits in the 1996 *Black Diamond Urban Growth Area Agreement (BDUGAA)* Section 6.4.1. It is a requirement of city Ord. 946 Condition 138, Ord. 970 Recital B(e), Ord. 970 Section 11.8, and Section 13.6.4.h.

3. It is a problem if housing does not come with services and infrastructure.

In Washington state, with our planning statutes, we are accustomed to having infrastructure where housing is built. Infrastructure costs and needs are not equal everywhere. Some larger cities already have transit and jobs and schools that can accommodate a large amount of growth. They have water and sewer systems ready. They have sports fields, city hall buildings, and fire stations.

It’s expensive and difficult to build this infrastructure, and it doesn’t happen overnight.

This is part of the reason that Black Diamond has a growth target that is not as big as the Ten Trails plan. Black Diamond’s “growth target” under the Growth Management Act is 2,900 new housing units and 680 jobs by the year 2044 (*King County Countywide Planning Policies* 2021). This is a massive amount of growth for Black Diamond, well more than double today’s housing. Black Diamond’s growth will impact state and County infrastructure that the developer is NOT paying for. In-city costs are also not fully funded by the developer. Residents of Black Diamond, King County and the state subsidize schools, roads, stormwater, and more.

4. Trying to “fix” the housing affordability crisis by adding dense housing to the city outside the MPD, as opposed to enforcing the MPD, will exacerbate problems.

***Given the city’s lack of enforcement of developer agreements, we should examine our assumptions.*** One assumption is that in the name of affordable housing, the city might zone for even more housing. Black Diamond doesn’t have a lot of jobs and it is not going to be served by significant transit now or in the future.



## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

Some people live in Black Diamond because they enjoy living away from the big city. Some people move out here because it's distance from job centers and its lack of infrastructure are exactly what makes it cheaper to buy here. If you are already struggling to enforce infrastructure requirements for Ten Trails, then how can we accommodate more people? It's not fair to people of limited income to add even more housing but not have enough schools, a degraded environment, and be stuck in traffic.

What can we do to enforce the Ten Trails requirement to make sure that 20% of its units are affordable as defined by King County's affordability metrics? We haven't heard anything from the city staff about these requirements even when the Council discusses concerns that there be affordable housing.

The city should not accept any further subdivision applications while the Developer is in violation of the Development Agreement.

## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

1996 *Black Diamond Urban Growth Area Agreement (BDUGAA)*, page 1 below shows the contracting parties. Page 10 follows showing Section 6.4.1 on affordable housing.

### **BLACK DIAMOND URBAN GROWTH AREA AGREEMENT**

THIS AGREEMENT is entered into by and between KING COUNTY, a Washington home rule charter county, the CITY OF BLACK DIAMOND, a Washington municipal corporation, PLUM CREEK TIMBER COMPANY, L.P. and PALMER COKING COAL COMPANY.

WHEREAS King County is a home rule charter county under the laws of the State of Washington with authority to enact laws and enter into agreements to promote the health, safety and general welfare of its citizens, including land use plans and development regulations, annexation agreements, and development agreements; and

WHEREAS the City of Black Diamond is a municipal corporation incorporated under the laws of the State of Washington with authority to enact laws and enter into agreements to promote the health, safety, and welfare of its citizens and thereby to control the use and development of property within its jurisdiction and to annex territory and specify zoning and development standards for annexed areas; and

WHEREAS Plum Creek Timber Company, L.P. and Palmer Coking Coal Company are owners of property within and in the vicinity of the Urban Growth Area for the City of Black Diamond; and

WHEREAS the King County Countywide Planning Policies ("CPPs") designated Joint Planning Areas for those cities, including Black Diamond, where agreement on the boundaries of each city's urban growth area had not been reached and required that they be designated by the end of 1995 or be subject to ratification; and

WHEREAS the CPPs designated a maximum 3000-acre Joint Planning Area (JPA) for the City of Black Diamond, within which a UGA could be designated by King County; and

WHEREAS in Ordinance 12065, adopted in December 1995, the King County Council designated a 1927-acre UGA for the City of Black Diamond, shown on the King County Comprehensive Plan Land Use Map as "New Rural City Urban Growth Area," which includes 1767 acres of the former JPA and 160 acres in the area known as Lake 12 Neighborhood; and

WHEREAS Ordinance 12065 also provided that by December 31, 1996 the following must occur or the New Rural City Urban Growth Area shall expire and automatically revert to a Rural designation: the King County Council must designate up to 915 acres of the lands within the New Rural City Urban Growth Area for future urban development and the remainder, excluding the Lake 12 Neighborhood, according to specified ratios, as Open Space or Natural Resource Use lands; and King County, the City of Black Diamond, and the affected property owners must address and resolve in a Potential Annexation Area and/or development agreement the issues delineated in Section 3 of the Ordinance; and

WHEREAS the CPPs establish policies for designating City potential annexation areas within the countywide urban growth boundary and require each City with a potential annexation area to enter into an interlocal agreement with the County for defining service delivery responsibilities; and



## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

1996 *Black Diamond Urban Growth Area Agreement (BDUGAA)* Section 6.4.1 on affordable housing.

**5.7 Phasing Areas within West Annexation Area.** The West Annexation Area may be annexed in three phases comprising the north area in Sections 2 and 3, the west area in Section 15, and the south area in Section 27 if approved by the City and County Councils; provided that for any such phasing the City and County Councils must determine the proportional provision of County, In-City and UGA Open Space required in Section 5.2(c) to fulfill the four to one requirements of open space to urban development; and provided further that infrastructure to the areas to be annexed must be provided consistent with Section 5.2(b).

### **6. URBAN DEVELOPMENT IN THE ANNEXED LANDS**

**6.1 Development Agreement.** On or before annexation of the West and South Annexation Areas, the City shall enter into a development agreement with Plum Creek which shall establish land uses, zoning and development standards for the Urban Development Areas in the West and South Annexation Areas consistent with the standards and service levels set forth below.

**6.2 Land Uses and Zoning.** Upon annexation of the West and South Annexation Areas, the City shall adopt land use designations and zoning for the Urban Development Areas that will permit the land uses as shown on Appendix A, Map 7; provided that the City and Plum Creek may agree to such other mixes of urban land uses within the Urban Development Areas of the West and South Annexation Areas as may be necessary to respond to real estate market and finance conditions. Upon annexation of the East Annexation Area, the City shall adopt land use designations and zoning that will permit urban residential development in this area. Upon annexation of the Lake 12 Annexation Area, the City shall adopt land use designation and zoning that will permit urban residential development at one dwelling unit per acre in the Lake 12 Annexation Area.

**6.3 Residential Density.** Following annexation, the West, South and East Annexation Areas shall be developed at a minimum average residential density of four units per acre. Maximum densities shall be determined prior to annexation as part of a development agreement between the City and property owners. Innovative planning, zoning and design shall be used to provide a variety of housing types serving all market segments. The base residential densities shall be two dwelling units per acre. The additional zoned residential density shall be achieved through participation in the City's TDR Program.

In the event development rights cannot be purchased through the City's TDR Program from either a private party or the TDR bank, or it is demonstrated to the City that, despite commercially reasonable efforts, development rights cannot be purchased on terms and conditions that allow annexation or development approvals to proceed, In-City Open Space created through other mechanisms consistent with Section 7.3 of this Agreement may be used as a density credit in the subject receiving area.

**6.4 Development Standards.** Following annexation the West, East, South, and Lake 12 Annexation Areas shall be developed under the jurisdiction of the City and shall at a minimum comply with the following County standards:

**6.4.1 Affordable Housing.** The parties will work with other local governments and appropriate agencies to maintain Black Diamond's fair-share of affordable housing and in that regard will take into account the City's existing housing stock.

(5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

King County Ordinance 12065. Pg. 1 for background:

December 15, 1995 g:\comp-pla\95Amend\bdordstk.doc	Introduced by: Chris Vance
December 19, 1995 clerk	Proposed No.: 95-791
1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44	<p>ORDINANCE NO. <b>12065</b></p> <p>AN ORDINANCE related to comprehensive planning, implementing recommendations relative to the joint planning area for the City of Black Diamond identified on the Growth Management Planning Council's Countywide Growth Pattern map; amending the King County Comprehensive Plan, King County Comprehensive Plan Land Use Map and Zoning Atlas.</p> <p>FINDINGS:</p> <ol style="list-style-type: none"><li>1. Joint Planning Areas (JPA) were designated by the Growth Management Planning Council (GMPC) in Countywide Planning Policy (CPP) FW-1 Step 8b for those cities, including Black Diamond, where agreement on the boundaries of each city's urban growth areas (UGA) had not been reached. The GMPC determined that the UGA for each city should be finalized by the end of 1995 and that, subject to conditions, the urban growth area/open space for Black Diamond could be designated on up to 3000 acres maximum.</li><li>2. Consistent with CPP FW-1 Step 8, King County Comprehensive Plan (KCCP) Policy I-206 states that the county will complete a joint planning process with the cities of North Bend, Issaquah, Redmond, Renton, Black Diamond and Snoqualmie to determine the final UGA boundaries for each city.</li><li>3. This ordinance is required to amend the boundaries of the UGA for the city of Black Diamond in accordance with the recommendations of the Executive's 1995 KCCP Amendments.</li></ol> <p>BE IT ORDAINED BY THE COUNCIL OF KING COUNTY:</p> <p><u>SECTION 1.</u> The overlay designation for the Black Diamond Joint Planning Area (as shown on the attached map) shall be deleted from the Countywide Growth Pattern map.</p> <p><u>SECTION 2.</u> The King County Comprehensive Plan, King County Comprehensive Plan Land Use Map and Zoning Map shall be amended as follows:</p> <p>A. The 783 acres of land annexed to the City in 1994 shall be included within the permanent Urban Growth Area (UGA) for the City as shown on Attachment A and as specified in the 1994 King County Comprehensive Plan text shall be designated "Incorporated City".</p> <p>B. 1,927 acres, including 1,767 acres of the former JPA and 160 acres in the area known as Lake 12 Neighborhood shall be designated "New Rural City Urban Growth Area" on the King County Comprehensive Plan Land Use Map as shown on Attachment A provided that no more than 915 acres, which does not include the area known as the Lake 12 Neighborhood.</p>



## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

King County Ordinance 12065. Pg. 3 affordable housing requirement.

12065

1           G. No annexations or extension of utilities or commitments for extension of utilities  
2 shall be allowed within the New Rural City Urban Growth Area until the proviso in  
3 subsections B, C, D and E above and the requirements of Sections 3 and 4 below are satisfied  
4 on or before December 31, 1996 in the Potential Annexation Area agreement and or  
5 development agreement as described in Section 4.

6           SECTION 3. King County, the City of Black Diamond and the affected property  
7 owners will address and resolve the following issues in the Potential Annexation Area  
8 agreement and or development agreement as described in Section 4:

9           A. Affordable housing that meets or exceeds the goals established by the Countywide  
10 Planning Policies and King County Comprehensive Plan. Market rate housing goals shall also  
11 be established.

12           B. Completion and/or amendment of the Black Diamond Comprehensive Plan that is  
13 consistent with the Countywide Planning Policies and includes:

14               1. The completion of the Natural Resource Management Plan, with input from King  
15 County Surface Water Management Division;

16               2. The UGA, when combined with the existing City boundary, provides a future  
17 job/housing mix sufficient for a fiscally viable city;

18               3. The net residential land within the New Rural City UGA (a portion of the 915  
19 acres) will have a base density to be determined but will be developed at a minimum of 2  
20 dwelling units per acre to a maximum of 18 dwelling units per acre through clustering.  
21 Transfer of Development Rights and other methods as described in the City's Comprehensive  
22 Plan; and

23               4. A Transfer of Development Rights program and residential densities that provide  
24 sufficient value to meet the open space goals within the existing City Limits

25           C. Nature and location of open space uses including resource management and the  
26 purposes described in the 1994 King County Comprehensive Plan found on pages 220-221.

27           D. Timing of open space and density transfers.

## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

*Ord. 946 The Villages MPD, Ex. C - Conditions of Approval Condition 138:*

“138. The project shall include a mix of housing types that contribute to the affordable housing goals of the City. The Development Agreement shall provide for a phase-by-phase analysis of affordable housing Citywide to ensure that housing is being provided at affordable prices. Specifications for affordable housing needs within the project shall be determined as a result of the phase-by-phase analysis.”

*Ord. 970, Development Agreement, Recital B(e), pg. 1-2:*

“...RCW 36.70B.170(3) defines "development standards" for a development agreement as including:

...(e) Affordable housing; ”

*Ord. 970, Development Agreement, Section 11.8, pg. 107-108:*

“11.8 HOUSING TYPES

Targets for housing types in each Phase of The Villages MPD are shown in Table 4-8-4. These are only targets not requirements. Pursuant to Condition of Approval No. 138 of the MPD

Permit Approval, after each Phase of The Villages MPD is completed, the City shall prepare an analysis of affordable housing City-wide. For purposes of this Agreement, Dwelling Units shall

be deemed "affordable housing" if the upper median income limits as determined by King County are satisfied. That analysis may be used to set specifications for affordable housing in any on-going or future Phase of The Villages MPD. Specifications for affordable housing needs within the MPD shall be determined as a result of the Phase-by-Phase analysis and shall be applied to implementing Projects prospectively.”

*Ord. 970, Development Agreement, Section 13.6 item 4.h, pg. 133:*

h. Affordable units are provided through the diverse mix of product types for the Implementing Projects, subject to the review and other requirements set forth in the Condition of Approval Nos. 138 and 139, and Agreement section 11.8.



## (5) Highway 169 fixes delayed while Ravensdale Rd and Roberts Dr intersections failing

### CONCLUSION

It's time to hold the developer to their agreement.

If the city does what it is obligated to do and stops allowing subdivisions or building permits, I think we will all be amazed at how quickly OakPointe will come into compliance/ Right now, OakPointe is splitting their resources toward things like new subdivision applications, and trying to change their Lawson Hills development plan. OakPointe's time should be solely spent figuring out how to come into compliance and stop violating the agreement.

All the requirements listed above were in exchange for OakPointe getting a big development approval.

The Development Agreement was supposed to ensure that public services needed for growth are provided. For most people, the existence of these services is something that they take for granted. Our state's growth management laws and other regulations usually work so well that public services and infrastructure are something we no longer tend to worry about. Our environmental protection laws usually work well enough that we don't have development polluting streams or filling wetlands.

Most people in Black Diamond don't realize how precarious these public services have become. They're counting on their city government to make sure growth doesn't overwhelm services. Here in Black Diamond, many things are getting missed and these are going to cost the community.

+++++

### RECOMMENDATIONS

While the developer is in violation of the Development Agreement Ordinance, the city should stop accepting new subdivision applications.

The City should notify the developer that they must immediately comply, or submit an amendment application regarding each violation and prepare for the public hearing and City Council review in accordance with the Master Planned Development change process as defined by city ordinance.

## Alex Campbell

---

**From:** Lisa Winters <ladygreyreiver@comcast.net>  
**Sent:** Wednesday, May 31, 2023 5:17 PM  
**To:** Kristen Bryant  
**Cc:** Friends BlackDiamond; Alex Campbell  
**Subject:** Re: Comments Pre Plat 2F PLN23-0019/PLN23-0020

Mr. Campbell,

I incorporate by reference the Friends of Black Diamond comments as my own.

Lisa Winters

On May 31, 2023, at 8:54 AM, Kristen Bryant <kristenbry@gmail.com> wrote:

Hello Mr. Campbell,

I incorporate by reference the Friends of Black Diamond Comments as my own.

Please also incorporate them from my father William Bryant of 25100 Roberts Drive. He does not normally use email, so please add him as a party of record at his physical address.

Thank you,  
Kristen Bryant

----- Original Message -----

From: Friends BlackDiamond <[friendsofblackdiamond@comcast.net](mailto:friendsofblackdiamond@comcast.net)>  
To: Alex Campbell <[acampbell@blackdiamondwa.gov](mailto:acampbell@blackdiamondwa.gov)>

Date: 05/31/2023 8:45 AM PDT  
Subject: Comments Pre Plat 2F PLN23-0019/PLN23-0020

To Alex Campbell and the City of Black Diamond Master Developer Review Team,

Attached are comments from Friends of Black Diamond and the individuals below on the Preliminary Plat "notice of application" on the city's public notice page at <https://www.blackdiamondwa.gov/public-notices/files/noa-pln23-0019>.

The notice indicates we can "request to receive notice of the public hearing, participate in any hearing, request a copy of the decision when it becomes available, and be made aware of any appeal rights," and that there will be additional opportunities to comment. We request to be advised of all of these via the email.

Thank you,  
Friends of Black Diamond and  
Kristen Bryant  
William Bryant  
Angela Fetting



Mike Fettig  
Gary Davis  
Lisa Winters  
Sherrie Jones  
Gary Jones  
Renee Mix

## Alex Campbell

---

**From:** Angela Rossman Fettig <angimal80@hotmail.com>  
**Sent:** Wednesday, May 31, 2023 6:55 PM  
**To:** Kristen Bryant  
**Cc:** Friends BlackDiamond; Alex Campbell  
**Subject:** Re: Comments Pre Plat 2F PLN23-0019/PLN23-0020

Mr. Campbell.

I incorporate by reference the Friends of Black Diamond comments on Plat 2F as my own.

Angela Fettig  
25423 Kanasket Drive  
Black Diamond, WA 98010

Sent from my iPhone

On May 31, 2023, at 8:54 AM, Kristen Bryant <kristenbry@gmail.com> wrote:

Hello Mr. Campbell,  
I incorporate by reference the Friends of Black Diamond Comments as my own.

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Thank you,



Friends of Black Diamond and  
Kristen Bryant  
William Bryant  
Angela Fettig  
Mike Fettig  
Gary Davis  
Lisa Winters  
Sherrie Jones  
Gary Jones  
Renee Mix

**TEN TRAILS MPD**

**PHASE 2 PLAT F PRELIMINARY PLAT**

**PLN23-0019**

**EXHIBIT 3**

**TITLE:** SEPA Mitigated Determination of Non-Significance  
(MDNS) for the Ten Trails MPD Phase 2 Plat F Preliminary Plat

**PREPARED BY:** The City of Black Diamond

**DATE:** October 4, 2023





# CITY OF BLACK DIAMOND

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Physical Address: 24301 Roberts Drive  
Mailing Address: PO Box 599  
Black Diamond, WA 98010

Phone: (360) 851-4500  
Fax: (360) 851-4501  
[www.blackdiamondwa.gov](http://www.blackdiamondwa.gov)

## State Environmental Policy Act (SEPA) Threshold Determination Mitigated Determination of Non-Significance (MDNS)

**Date of issuance:** October 4, 2023

**Lead agency:** City of Black Diamond Community Development

**Agency Contact:** Mona Davis, [mdavis@blackdiamondwa.gov](mailto:mdavis@blackdiamondwa.gov), 360-851-4528

**Agency File Number(s):** PLN23-0019 Preliminary Plat / PLN23-0020 SEPA Review

**Project Name:** Ten Trails MPD Phase 2 Plat F Preliminary Plat

**Description of proposal:** Applicant requests Preliminary Plat approval to subdivide four parcels comprising of approximately 44.27 acres into 123 lots, which would provide a mix of 77 detached single-family lots, four attached single-family lots, and 42 multi-family lots. Approximately 28.93 acres of open space will be provided in tracts within the proposed subdivision, which will consist of a neighborhood park, pocket park, open space, trails, natural landscape, and sensitive area uses. No wetland impacts will occur; however, the project proposes wetland buffer averaging to mitigate for wetland buffer impacts.

**Location of proposal:** The project site is in the City of Black Diamond, consisting of four King County Tax Parcels 222106-9035, 222106-9041, 222106-9052, and 222106-9053; NE ¼ of Section 22, Township 21 N, Range 6E WM.

**Applicant(s):** CCD Black Diamond Partners LLC / Oakpointe  
3025 – 112<sup>th</sup> Ave NE, Suite 100, Bellevue, WA 98004

The City of Black Diamond (Lead Agency) has determined that this proposal will not have a probable significant adverse impact on the environment. Pursuant to WAC 197-11-350(3) and WAC 197-11-355(4), the proposal has been clarified, changed, and conditioned to include necessary mitigation measures to avoid, minimize or compensate for probable significant impacts. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c).

This determination is based on the comments received during the Notice of Application comment period (5-17-23 to 6-1-23) and the following findings, conclusions, mitigation measures, and permit conditions are required to adequately address potential impacts from this project proposal. All relevant materials related to this file are available to the public by request to the city contact listed above.

**Mitigation Measures:** The following mitigation measures are hereby incorporated into this Mitigated Determination of Non-Significance (MDNS). These conditions are in addition to the mitigation required from development regulations and other conditions resulting from other government approvals.

1. Prior to the commencement of any site disturbance activities, an approved National Pollutant Discharge Elimination System (NPDES) General Stormwater Construction Permit issued by the Washington State Department of Ecology covering the proposed scope of work shall be submitted to the City's Master Development Review Team (MDRT). BMPs outlined in the Geotechnical Engineering Report, prepared by Associated Earth Sciences revised September 7, 2023, shall be implemented throughout construction to mitigate potential adverse impacts from erosion hazards.
2. Prior to the commencement of any site disturbing activities, a Spill Prevention Control and Countermeasures Plan (SPCCP) shall be submitted to, and approved by, the City's MDRT.
3. Prior to any site disturbance activities during the City's Winter Work period (October 1 through March 31), a Winterization Plan pursuant to Black Diamond Engineering Design and Construction Standards Section 2.2.05 shall be submitted to, and approved by, the City's MDRT prior to commencing construction.
4. Wetland monitoring protocols and methodology established in Section 1.4.1 of the Sensitive Area Study & Buffer Averaging Plan (prepared by Wetland Resources, Inc. and dated September 7, 2023) shall be submitted to, and approved by, the City's MDRT prior to commencing construction. Mitigation plantings shall meet the monitoring provisions of BDMC 19.10.260.
5. Trail alignments within sensitive area buffers shall be flagged and demarcated by the applicant for City review prior to any trail section vegetation disturbance to verify the route of least disturbance to forested areas and native vegetation. The applicant shall submit protocols for trail alignment demarcation, City verification, and construction to minimize vegetation disturbance that is consistent with the provisions of BDMC 19.10.220(B)(3). No vegetation clearing is to occur beyond the approved clearing limits.

**Signature:** Mona Davis **Date:** 10-4-2023  
Community Development Director/SEPA Responsible Official

**Public Comment Period:** This MDNS is issued under WAC 197-11-350. The Lead Agency will not act on this proposal for 14 days from the date of this decision. Written comments must be submitted no later than 5:00 pm on October 18, 2023 to the City of Black Diamond Community Development Department, 24301 Roberts Drive, Black Diamond, WA 98010.

**SEPA Appeal Period:** There is a 14-day appeal period regarding this agency decision. You may appeal this determination no later than 5:00 pm on October 18, 2023 by completing the proper appeal form and paying the City's appeal fee. You should be prepared to make specific factual objections in your appeal. Contact the Community Development Department at (360) 851-4447 to ask about the procedures for SEPA appeals.





**EXHIBIT 4**

**TITLE:** Traffic Impact Study for the Ten Trails MPD Phase 2 Plat F Preliminary Plat

**PREPARED BY:** TranspoGroup on behalf of Oakpointe LLC

**DATE:** October 25, 2023

## TECHNICAL MEMORANDUM

Date:	October 25, 2023	TG:	16450.00
To:	Brian Ross and Justin Wortman – Oakpointe		
From:	Mike Swenson, P.E., PTOE and Maris Fry, P.E. – Transpo Group		
Subject:	Ten Trails MPD – Phase 2 Plat F Traffic Impact Study		

This memo serves as a Traffic Impact Study (TIS) for Plat F of Phase 2 (“Plat 2F”) of the Ten Trails Master Planned Development<sup>1</sup> (MPD) and supplements the analyses conducted in the *Ten Trails and Lawson Hills MPDs – Phase 2 Traffic Monitoring Report* (the “*Phase 2 TMR*”) and the *Ten Trails and Lawson Hills MPDs – Phase 1A Mid-Point Traffic Monitoring Report* (the “*Phase 1A MP TMR*”). It provides a description of Plat 2F as well as estimates of weekday PM peak hour vehicle trip generation. It also determines the off-site improvements that would be triggered as part of Plat 2F, discloses potential impacts to traffic safety, addresses anticipated traffic impacts associated with construction activities, and describes the traffic calming measures internal to Plat 2F.

Exhibit F of The Villages MPD Development Agreement requires the preparation of a TMR for each phase of the combined Ten Trails and Lawson Hills MPDs prior to submittal of implementing project applications for such phase. The *Phase 2 TMR* was submitted to the City of Black Diamond for Phase 2 of the combined Ten Trails and Lawson Hills MPDs in March 2011. As such, this Exhibit F requirement for Phase 2 implementing projects has been satisfied.

As required by Exhibit F, the *Phase 2 TMR* identified Phase 2 traffic impacts and the improvements triggered by such impacts along with their equivalent residential unit (ERU) thresholds necessary to maintain the City of Black Diamond, King County and Washington State Department of Transportation (WSDOT) adopted level of service (LOS) standards. That said, the *Phase 1A MP TMR* was completed more recently than the *Phase 2 TMR* and analyzes the cumulative impacts of Phase 1A, 1B and 2 of the combined MPDs. This TIS analyzes how the weekday PM peak hour vehicles trips generated by the cumulative Ten Trails and Lawson Hills MPDs through build-out of Plat 2F trigger any of the ERU thresholds for improvements set forth in the *Phase 1A MP TMR*.

This study contemplates the cumulative development of Phases 1A, 1B and 2 of the Ten Trails and Lawson Hills MPDs at build-out of Plat 2F, which is expected to occur in 2026. Through 2026, the cumulative Ten Trails and Lawson Hills MPDs are projected to generate approximately 2,110 net new weekday PM peak hour trips, with approximately 89 of those peak hour trips resulting from Plat 2F.

## Project Description

The subject plat is located within Phase 2 of the Ten Trails MPD, which is located south of Roberts Drive and south of Phase 1A. Vehicular access to Plat 2F would be provided via Ten Trails Parkway SE and a set of internal roadways and alleys. The subject plat would construct a mix of age-qualified dwelling units, single-family dwelling units, and multi-family dwelling units.

The federal Housing for Older Persons Act (HOPA) exempts senior housing against familial status discrimination provided at least eighty (80) percent of the homes in a community are occupied by at least one person aging from 55 and above. To better align with HOPA and other fair housing

<sup>1</sup> Ten Trails MPD was formerly known as The Villages MPD until September 2016.



laws, non-age-qualified occupants may occupy an age-qualified unit within Plat 2F if, at the time of application, at least 85% of the remaining occupied units are occupied by at least one age-qualified occupant. The proposed development program for Plat 2F includes 77 single-family age-qualified dwelling units, 4 single-family dwelling units and 42 multi-family dwelling units, but to reflect the proposed 85% threshold, it is assumed that Plat 2F will effectively include 65 single-family age-qualified dwelling units, 16 single-family dwelling units, and 42 multi-family dwelling units. As the age-qualified units will be marketed as a 55+ community and the CCRs require at least the initial occupation of all units within the community by an age-qualified occupant, this is a very conservative assumption that will likely never be realized.

The proposed 85% threshold assumption is reflected in all age-qualified units within Phase 2 such that Phase 2 effectively includes a total of 262 age-qualified residential units rather than the 309 age-qualified units, as currently proposed. This TIS looks at what off-site improvements would be triggered by Plat 2F in conjunction with these changes, discloses potential impacts to traffic safety, addresses anticipated traffic impacts associated with construction activities, and describes the traffic calming measures internal to Plat 2F.

A site plan for Plat 2F is provided in Attachment 1.

## Project Trip Generation

Weekday PM peak hour vehicle trip generation for Plat 2F was estimated based on the size and type of development and assumptions outlined in the *Trip Generation Manual* (Institute of Transportation Engineers (ITE), 11<sup>th</sup> Edition) and *Trip Generation Handbook* (ITE, 3<sup>rd</sup> Edition). This methodology was chosen as it is consistent with the approach used to estimate trips for *The Villages Transportation Technical Report (TTR)* (Parametrix, December 2009) and the *Phase 1A MP TMR* which determined the trip count thresholds that triggered specific intersection improvements.

The *Trip Generation Manual* has been updated since *The Villages TTR* was published in December 2009. Weekday PM peak hour trip regression equations from the most recent edition of the *Trip Generation Manual* were used for all land uses in estimating vehicle trip generation, consistent with the approach and methodology used for *The Villages TTR*. Since trip regression equations were used, the number of trips per dwelling unit generally decrease as the number of units increase; an effective trip rate for the trips per dwelling unit was back-calculated based on the trip regression equation.

The land use assumptions and trip generation estimates for the Plat 2F land use proposal is summarized in Table 1. As shown, Plat 2F is expected to generate a total of 89 weekday PM peak hour vehicle trips, approximately 89 ERUs<sup>2</sup>. Detailed trip generation calculation worksheets are included in Attachment 2.

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<sup>2</sup> One ERU was assumed to be equal to one weekday PM peak hour trip, consistent with the rate used to identify the thresholds for the off-site intersection improvements in previous studies.

**Table 1. PM Peak Hour Project Trip Generation Estimates – Plat 2F**

Land Use	Unit <sup>1</sup>	Size	Gross Trips
			Total (In/Out)
Single-Family Residential	DUs	16	18 (11/7)
Multi-Family Residential	DUs	42	39 (25/14)
Age-Qualified Residential	DUs	65	32 (20/12)
<b>Total</b>			<b>89 (56/33)</b>

1. DUs= dwelling units

## Timing of Off-Site Improvements

This section describes the timing of improvements based on the ERU thresholds for improvements outlined in the *Phase 1A MP TMR*. As noted within the *Phase 1A MP TMR*, improvements are outlined for all intersections expected to operate below the applicable LOS standard by Phase 1A build-out, which is inclusive of Plat 2F build-out. Intersections excluded from the list of needed intersection improvements include those that have already been implemented or are planned to be complete prior to Phase 1A build-out<sup>3</sup>. As such, the timing of improvements outlined in the *Phase 1A MP TMR* includes intersections that were included in the *Detailed Phase 2 Implementation Schedule* but were reevaluated as part of the *Phase 1A MP TMR* based on the updates to the development program and absorption schedule of the collective MPDs. Most notably, the *Phase 1A MP TMR* evaluated the impacts of the MPDs inclusive of Phase 1A, Phase 1B and Phase 2, whereas the ERU triggers outlined in the *Detailed Phase 2 Implementation Schedule* only outlines impacts associated with Phase 1A and Phase 2. Additionally, further coordination with WSDOT regarding the scope of improvements along SR 169, background improvements at study intersections, and minor shifts to baseline travel patterns in the area have impacted the scope and timing of some intersection improvements.

The timing of improvements outlined in the *Phase 1A MP TMR* describes the transportation-related improvements which the Master Developer is conditioned to construct at certain ERU thresholds. The ERU thresholds represent the cumulative ERUs for Phases 1A, 1B and 2 of the overall Ten Trails and Lawson Hills MPDs. To determine which improvements would be necessary by build-out of Plat 2F, the combined trip generation of Phases 1A, 1B and 2 was determined for the year in which Plat 2F is expected to be complete (2026). The same trip generation methodology and assumptions were used to determine the cumulative trip generation as described above for the Plat 2F trip generation. Detailed trip generation calculation worksheets are included in Attachment 2.

As shown in Table 2, Phases 1A, 1B and 2 at the build-out of Plat 2F would generate 2,110 net new weekday PM peak hour trips (2,110 ERUs).

<sup>3</sup> Improvements that have been implemented to-date include rechannelization of SE 288th Street/216th Avenue SE to provide a left-turn refuge/merge lane, a single-lane roundabout at Lake Sawyer Road SE/Roberts Drive, and two new site access intersections – a single-lane roundabout at Roberts Drive/Ten Trails Parkway and a stop-controlled intersection at Roberts Drive/Ten Trails Place. Improvements at SR 169/SE 288th Street (eastbound-to-northbound refuge/acceleration lane), SR 169/Roberts Drive (single-lane roundabout with turn lanes) and SR 169/Pipeline Road (single-lane roundabout with turn lanes) are also planned to be complete prior to build-out of Plat 2F.



**Table 2. 2026 PM Peak Hour Cumulative Trip Generation Estimates – Phase 1A, 1B and 2**

Land Use	Unit <sup>1</sup>	Size	Gross Trips	Internal Trips	Pass-by Trips	Net New Trips
			Total (In/Out)	Total (In/Out)	Total (In/Out)	Total (In/Out)
<u>Ten Trails MPD<sup>2</sup></u>						
Single-Family Residential	DUs	1,240	1,059 (667/392)	195 (138/57)	-	864 (529/335)
Multi-Family Residential	DUs	438	209 (132/77)	33 (24/9)	-	176 (108/68)
Age-Qualified Residential	DUs	262	94 (57/37)	15 (11/4)	-	79 (46/33)
Elementary School	Students	600	96 (44/52)	29 (13/16)	-	67 (31/36)
Office	KSF	8	20 (3/17)	5 (2/3)	-	15 (1/14)
Retail	KSF	271	1,157 (555/602)	217 (59/158)	188 (94/94)	752 (402/350)
<u>Lawson Hills MPD<sup>3</sup></u>						
Single-Family Residential	DUs	106	105 (66/39)	-	-	105 (66/39)
Multi-Family Residential	DUs	72	52 (33/19)	-	-	52 (33/19)
Total			2,792 (1,557/1,235)	494 (247/247)	188 (94/94)	2,110 (1,216/894)

1. DUs= dwelling units; KSF=1,000 square feet

2. The Ten Trails MPD would include development associated with Phase 1A, 2, and 1B at build-out of Plat 2F.

3. The Lawson Hills MPD would include development associated with Phase 2 only at build-out of Plat 2F.

Based on timing of improvements outlined in the *Phase 1A MP TMR*, the following intersections would be triggered for improvement prior to build-out of Plat 2F. An overview of the proposed improvements and ERU triggers at these intersections is included in Table 3.

- SE 288th Street/216th Street
- SE Covington-Sawyer Road/216th Avenue SE
- SR 169/Baker Street
- SR 169/Lawson Street
- Roberts Drive/Ten Trails Place
- Lake Sawyer Rd/Ten Trails Pkwy SE
- SR 169/SE 288th Street

**Table 3. Summary of Intersection Improvements and Construction Timing**

Intersection	Improvement	Commence construction prior to City's issuance of a certificate of occupancy for <sup>1</sup>
SE 288th St/216th Ave SE	Traffic Signal + NBR Turn Lane	827th ERU
SE Covington-Sawyer Rd/216th Ave SE	NBL Turn Lane	1,089th ERU
SR 169/Baker St ( <i>First Phase</i> )	EB-to-NB Refuge/Merge Lane + NBL Turn Lane	1,089th ERU
SR 169/Lawson St ( <i>First Phase</i> )	NBL/SBL Turn Lanes	1,089th ERU
Roberts Dr/Ten Trails Pl SE	Traffic Signal	1,422nd ERU
SR 169/Baker St ( <i>Final Phase</i> )	Traffic Signal	1,422nd ERU
SR 169/Lawson St ( <i>Final Phase</i> )	Traffic Signal	1,422nd ERU
Lake Sawyer Rd/Ten Trails Pkwy SE	Roundabout	1,800th ERU
SR 169/SE 288th St	Traffic Signal	1,954th ERU

Note: ERU = equivalent residential unit; NBR = northbound right; NBL = northbound left; SBL = southbound left

1. The number of ERUs reflects the combination of Phases 1A, 1B and 2 within the Ten Trails and Lawson Hills MPDs

Of the improvements outlined in Table 3, the roundabout at Lake Sawyer Road/Ten Trails Parkway SE has already been constructed. Additionally, the traffic signal and channelization improvements at SE 288th Street/216th Avenue SE and the channelization improvements at SE Covington-Sawyer Road/216th Avenue SE are under construction.

## **Additional Transportation Improvements**

As it relates to the timeline of Plat 2F, Section 6.4.3 of *The Villages MPD Development Agreement* provides that the preliminary design and alignment of Pipeline Road shall be completed by the Master Developer and the right of way dedicated to the City prior to the City's approval of a building permit for the 1,200th building permit within the Ten Trails MPD. This section further provides that Pipeline Road shall be constructed by the Master Developer and open for traffic prior to the City's approval of a building permit for the 1,746th dwelling unit of the Ten Trails MPD or when the Traffic Monitoring Plan shows that construction is necessary to prevent a significantly adverse degradation of Level of Service on Roberts Drive.

Through build-out of Plat 2F, the Ten Trails MPD would total 1,940 dwelling units, surpassing this defined dwelling unit threshold. As such, dedication of right of way for and construction of Pipeline Road would be complete prior to build-out of Plat 2F. However, based on analysis provided in the *Black Diamond Transportation Model Update per Hearing Examiner Conditions* performed by Parametrix in March 2023, and the following *Pipeline Road – Sensitivity Analysis* performed by Parametrix in July 2023, Pipeline Road does not need to be constructed and open to traffic until after the 2,866th dwelling unit is constructed provided that improvements at Morgan Street/Roberts Drive are constructed by 2024. The City is currently processing a Minor Amendment to the Development Agreement that would alter the dwelling unit trigger to align with this revised analysis therefore the construction of Pipeline Road would not be triggered prior to build-out of Plat 2F, but the improvements at Morgan Street/Roberts Drive would be required prior to build-out of Plat 2F.

## **Traffic Safety**

Collision data for the most recent (pre-pandemic) three-year period were obtained from WSDOT and summarized at each intersection and along each roadway segment evaluated in the previous traffic studies for the Ten Trails and Lawson Hills MPDs.

### ***Intersection Collision Analysis***

Collisions at study intersections were determined using WSDOT's classification of the "Junction Relationship" of the collision as well as by reviewing collisions that were noted to occur within 200 feet of intersections. All collisions that were classified as "At Intersection and Related" or "Intersection Related but Not at Intersection" were included. Collisions adjacent to intersections were also reviewed to determine whether the cause was related to the intersection (e.g., rear-end type collisions) or related to the roadway (e.g., fixed-object collisions) and included in the collision summaries. The number of collisions reported at these intersections are summarized in Table 4.



**Table 4. Three-Year Collision Summary at Intersections – 2017 to 2019**

Intersection	2017	2018	2019	Total	Annual Average	Collisions per MEV <sup>1</sup>
SE 288th St/216th Ave SE	0	1	2	3	1.00	0.26
SE 288th St/232nd Ave SE	0	0	0	0	0.00	0.00
SE Covington-Sawyer Rd/216th Ave SE	5	3	3	11	3.67	0.84
SE Auburn-Black Diamond Rd/218th Ave SE	1	0	0	1	0.33	0.12
Roberts Dr/Lake Sawyer Rd SE	0	1	0	1	0.33	0.14
Roberts Dr/Morgan St	0	0	1	1	0.33	0.20
SR 169/SE 288th St	4	4	6	14	4.67	0.97
SR 169/SE Black Diamond-Ravensdale Rd	4	2	2	8	2.67	0.55
SR 169/Roberts Dr	1	1	1	3	1.00	0.21
SR 169/Baker St	3	0	2	5	1.67	0.35
SR 169/Lawson Rd	4	0	0	4	1.33	0.34
SR 169/Jones Lake Rd	1	0	0	1	0.33	0.09
SR 169/SE Green Valley Rd	1	3	1	5	1.67	0.44
SE Kent-Kangley Rd/Landsburg Rd SE	8	2	6	16	5.33	1.63
SE Auburn-Black Diamond Rd/SE Green Valley Rd	1	0	0	1	0.33	0.08

Source: WSDOT, 2020.

Note: Under 23 U.S. Code § 409 and 23 U.S. Code § 148, safety data, reports, surveys, schedules, lists compiled or collected for the purpose of identifying, evaluating, or planning the safety enhancement of potential crash sites, hazardous roadway conditions, or railway-highway crossings are not subject to discovery or admitted into evidence in a Federal or State court proceeding or considered for other purposes in any action for damages arising from any occurrence at a location mentioned or addressed in such reports, surveys, schedules, lists, or data.

1. Collisions per one million entering vehicles (No. of reported collisions x 1,000,000) / (Average daily traffic volumes x 365 x 3 years)

As shown in Table 4, the majority of intersections experienced an average of two collisions or fewer during this three-year period. Based on a review of collision severity of the 74 total collisions, 48 involved no injuries, 20 involved “possible injury,” four involved “suspected minor injury” and the remaining two involved “suspected serious injury.” There was one pedestrian collision and zero bicyclist collisions. The pedestrian collision occurred at the SR 169/Lawson Street intersection and resulted in a possible injury. There were no fatalities reported at study intersections.

The number of collisions per one million entering vehicles was also evaluated to provide a comparable rate between locations and determine if further evaluation of traffic safety is necessary. *The Transportation Impact Analyses for Site Development, An ITE Recommended Practice* (ITE, 2010) recommends the following procedure in evaluating traffic safety at intersections:

*The initial review of existing data within a study area should include recent (within 3 years) collision experience. This review should identify locations where transportation safety should be given extra consideration. High-collision locations (based on number, rate and severity) on roadways serving the study site should be analyzed. Collision rates vary, but any intersection with more than one collision per million entering vehicles (MEV) may be worthy of additional analysis. (page 74)*

Based on the data summarized in Table 4 above, only the SE Kent-Kangley Road/Landsburg Road SE intersection experienced a collision rate over 1.0 collisions per MEV, recording a collision rate of 1.63 collisions per MEV. Further evaluation of this intersection showed that all collisions were categorized as “entering at angle” and involved vehicles on both the stop-controlled north and southbound approaches colliding with vehicles traveling eastbound or westbound on SE Kent-Kangley Road. With respect to severity at SE Kent-Kangley Road / Landsburg Road SE, 10 collisions involved no injuries and 6 were reported as “possible injuries.” Thus, just under two-

thirds of all reported collisions resulted in no injuries. No pedestrians or bicyclists were involved in any of the collisions and no fatalities were reported as well.

To help address the elevated collision rate at this intersection, King County converted the intersection to all-way stop-controlled in the spring of 2021. Construction has also recently begun on an intersection improvement project that will include an eastbound right channelized right-turn lane and a traffic island with a new crosswalk. The County is monitoring operations and safety at this intersection as a result of the changes in intersection control, with intentions to install a permanent traffic calming safety improvement, such as a roundabout or traffic signal, in the future.

## Roadway Collision Analysis

Collision data along roadway segments in the City of Black Diamond from 2017-2019 was also obtained from WSDOT and analyzed. A summary of collision data during the most recent three-year period is summarized in Table 5.

**Table 5. Three-Year Collision Summary for Roadway Segment – 2017 to 2019**

Roadway Segments	2017	2018	2019	Total	Annual Average	Collisions per MVM <sup>1</sup>
SE ABD Rd (218th Ave SE to Lake Sawyer Rd SE)	1	0	1	2	0.67	0.28
Roberts Dr (Lake Sawyer Rd SE to Morgan St)	1	1	1	3	1.00	1.03
Roberts Dr (Morgan St to SR 169)	0	0	0	0	0.00	0.00
Morgan St (Roberts Dr to Baker St)	0	0	0	0	0.00	0.00
216th Ave SE (SE 288th St to SE 304th St)	7	4	3	14	4.67	1.22
Lake Sawyer Rd SE (SE 304th St to Roberts Dr)	0	2	1	3	1.00	0.71
SR 169 (SE 288th St to Roberts Dr)	6	3	3	12	4.00	0.54
SR 169 (Roberts Dr to Lawson St)	4	2	6	12	4.00	1.58
SR 169 (Lawson St to Jones Lake Rd)	0	3	4	7	2.33	1.90
SR 169 (Jones Lake Rd to SE Green Valley Rd)	0	3	2	5	1.67	0.36

Source: WSDOT, 2020.

Note: ABD = Auburn-Black Diamond

Note: Under 23 U.S. Code § 409 and 23 U.S. Code § 148, safety data, reports, surveys, schedules, lists compiled or collected for the purpose of identifying, evaluating, or planning the safety enhancement of potential crash sites, hazardous roadway conditions, or railway-highway crossings are not subject to discovery or admitted into evidence in a Federal or State court proceeding or considered for other purposes in any action for damages arising from any occurrence at a location mentioned or addressed in such reports, surveys, schedules, lists, or data.

1. Collisions per one million vehicle-miles traveled (No. of reported collisions x 1,000,000) / (Segment length x Average daily traffic volumes x 365 x 3 years)

As shown in Table 5, on average, approximately 5 collisions per year or less were reported along the roadway segments during the most recent three-year period. Based on a review of collision severity of the 58 total collisions, 41 involved no injuries, 12 involved “possible injury,” 3 involved “suspected minor injury” and 1 involved “suspected serious injury.” Thus, a majority of all reported collisions resulted in no or no apparent injuries. There was one fatality that occurred in July 2017 which involved a bicyclist and a truck (flatbed, van, etc.) on SR 169 approximately 0.6 miles south of SE 288th Street. The truck was reported to be traveling northbound; no other details are provided in the WSDOT collision report. There was also one pedestrian-related collision which occurred at a SR 169 intersection between Roberts Drive and Lawson Street. The vehicle was traveling westbound and failed to yield right of way to the pedestrian.

In addition to the annual collision data, the number of collisions occurring per one million vehicle miles (MVM) traveled was calculated for each segment and ranged from 0.00 to 1.90 collisions per MVM traveled. Four roadway segments experienced more than one collision per MVM traveled, including 216th Avenue SE between SE 288th Street and SE 304th Street, Roberts Drive between Lake Sawyer Rd SE and Morgan Street, SR 169 between Roberts Drive and Lawson Street, and



SR 169 between Lawson Street and Jones Lake Road. Common collision types for each roadway segment are as follows:

- 216th Avenue SE segment: there was not one cause/collision type that represented a significant portion of all collisions. The collision types varied significantly and ranged from left turns to fixed objects to right turns.
- Roberts Drive segment: all collisions were a result of vehicles colliding with fixed objects. These objects included a mailbox, tree and a fence.
- SR 169 segment between Roberts Drive and Lawson Street: the most common collision type was rear ends (4), followed by various other types such as approach turn, parked vehicle, right turn, and opposite direction.
- SR 169 segment between Lawson Street and Jones Lake Road: the most common collision type was rear ends and fixed object collisions (3).

By comparison, based on the rates presented in the *2015 Washington State Collision Data Summary*, the average collision rate was 2.48 collisions per MVM travelled in King County and 1.96 per MVM travelled in all of Washington State. Therefore, the collision history on these four segments of roadway are well below the Countywide and Statewide averages which capture collisions in urban, suburban and rural areas.

New traffic generated by the Ten Trails MPD would likely result in a proportionate increase in the probability of traffic collisions. It is unlikely, however, that this traffic would create a safety hazard or significantly increase the number of reported collisions. It is unlikely project traffic would exacerbate an existing traffic safety hazard because no roadway segments in and around Black Diamond experienced an unusually high collision rate during the most recent three-year period.

## **Construction Impacts**

This section addresses the anticipated traffic impacts associated with the construction activities for Plat 2F.

### ***Construction Phasing and Timing***

Construction traffic associated with the development of Plat 2F would be affected by phasing and timing of construction. Initial construction activities would include early clear and grade followed by earthwork and the installation of in-plat roads and utilities. This includes the grading of the site, installing roadways, and installation of necessary utilities such as power, water, and sewer. Following clear and grade, earthwork and installation of roads and utilities, the vertical construction of the residential dwelling units would occur.

The schedule for Plat 2F will vary based on the duration and type of work being completed. A summary of the tentative schedule for Plat 2F is shown in Table 6. A more detailed analysis of the construction phasing and timing is included in a memorandum prepared by David Evans and Associates and included as Attachment 3.

**Table 6. Planned Construction Schedule – Plat 2F**

<u>Early Clear &amp; Grade</u>		<u>Earthwork</u>		<u>Road and Utilities – In Plat</u>		<u>Vertical Construction</u>	
Dates	Duration	Dates	Duration	Dates	Duration	Dates	Duration
July–Oct. '23	4 months	Apr.–July '24	4 months	Apr.–Sept. '24	6 months	Dec. '24–May '26	18 months

Source: David Evans and Associates  
1. Lift Station and Offsite Utilities

As shown in Table 6, early clear and grade activities will begin in summer 2023 and are expected to be complete in fall 2023, over a period of 4 months. Earthwork activities for Plat 2F will begin in spring 2024 and are expected to be completed in summer of 2024, over a period of 4 months. In-plat road and utility construction is expected to begin in spring 2024 and is expected to be complete in fall 2024, after a period of 6 months. Vertical construction is expected to take place from winter 2024 to spring 2026, over a period of 18 months.

### ***Construction Trip Generation***

The amount of construction traffic associated Plat 2F was estimated for the number of truck trips as well as employee or crew trips. The construction timeline and schedule were also reviewed to understand the anticipated volume of daily construction traffic as well as during the weekday PM peak hour at adjacent street traffic (one-hour period of greatest demand between the hours of 4:00 p.m. and 6:00 p.m.).

The primary hours for construction are between 7:00 a.m. and 3:00 p.m. with most truck traffic avoiding delays associated with traffic congestion during the PM peak hour of adjacent street traffic; however, these calculations conservatively assume that some construction traffic would occur during the weekday PM peak hour as summarized below. In addition, it was assumed that each construction worker would arrive and depart in a single-occupant vehicle even though it is likely that some construction workers would carpool to/from the project site, effectively reducing the trip generation estimates identified in this memo.

A summary of the daily and weekday PM peak hour construction trips anticipated for Plat 2F is provided in Table 7. This data was derived from detailed trip generation calculations and construction assumptions provided in the construction memorandum provided by David Evans and Associates (Attachment 3). It should be noted that the data related to daily traffic from the David Evans Associates memorandum represents round trips, which were doubled in the table below to account for each entering and exiting trip end associated with daily trips.

**Table 7. Construction Trip Generation Summary – Ten Trails MPD Plat 2F**

<u>Construction Type</u>	<u>Weekday Daily Trips<sup>1</sup></u>			<u>Weekday PM Peak Hour Trips</u>		
	Crew	Truck	Total	Crew	Truck	Total
Early Clear & Grade	24	2	26	2	1	3
Earthwork	24	32	56	2	1	3
Road and Utilities	24	8	32	2	1	3
Vertical	76	6	82	6	1	7

1. Daily trips identified in the David Evans and Associates memorandum represent round trips / individual trip ends at the project site and were doubled in this memorandum to account for each entering and exiting trip end associated with a daily trip.

During early clear and grade, it is estimated that Plat 2F would generate approximately 24 daily crew trips and two daily truck trips with up to two crew trips and one truck trip occurring during the weekday PM peak hour of adjacent street traffic.



During earthwork construction, it is estimated that Plat 2F would generate approximately 24 daily crew trips and 32 daily truck trips with up to two crew trips and one truck trip occurring during the weekday PM peak hour of adjacent street traffic.

During road and utility construction, it is estimated that Plat 2F would generate approximately 24 daily crew trips and 8 daily truck trips with up to two crew trips and one truck trip occurring during the weekday PM peak hour.

During the vertical construction, it is estimated that Plat 2F would generate approximately 76 daily crew trips and 6 daily truck trips with up to 6 crew trips and one truck trip occurring during the weekday PM peak hour.

### ***Construction Impacts***

As shown in the previous sections, the highest weekday PM peak hour trip generation for Plat 2F would be during vertical construction. During this time, the project is estimated to generate approximately 7 trips during the PM peak hour. As the impacts for the buildout have been identified and the trip generation estimated for the construction traffic is significantly lower, no additional off-site mitigation should be necessary to mitigate traffic operations during the construction of Plat 2F.

### ***Construction Management Plan***

A construction management plan will be developed by the Master Developer in coordination with the City of Black Diamond to provide for a safe and efficient construction site and minimize the impacts to traffic operations in the area as required by Section 1.17 of the City of Black Diamond Engineering Design and Construction Standards. This plan will minimize off-site construction impacts through containing equipment, materials, and workers on-site as much as possible and accommodating staging, construction facilities and parking on-site. Specific transportation-related items anticipated to be addressed in a construction management plan include:

- **Truck Routes** – identifying specific haul routes for trucks, which will avoid impacts to local residential streets.
- **Noise** – minimizing noise impacts associated with construction on-site as well as from haul trucks on the roads.
- **Parking** – identifying parking areas for employees as well as staging areas for trucks and materials.
- **Access** – Identifying specific areas for access that would likely require safe controlled access for large trucks to and from the site.
- **Compute Trip Reduction** – encouraging carpooling and other ride sharing by employees to minimize the number of single occupant vehicle trips on site.

In addition, Sheet UA1 of the “Utility Availability” plan for Plat 2F includes preliminary haul route plans that have two plat notes that provide additional mitigation to minimize construction impacts to SE Green Valley Road and allow the City an opportunity to review and approve construction traffic control designs. The notes read as follows:

1. Master Developer shall include the following provision in clearing, grading and construction contracts: “Except for the westerly 1,000 feet of SE Green Valley Road, SE Green Valley Road shall not be used as a construction haul route by contractors or its agents.”

2. Construction traffic control design will be provided as part of final engineering plans for review and approval by the City of Black Diamond.

## Traffic Calming Measures

As part of the Ten Trails MPD, a variety of traffic calming measures will be incorporated within Plat 2F to calm traffic and help minimize excessive vehicle speeds.

Curb bulb-outs are one traffic calming measure which would narrow the roadway width by providing a physical constraint requiring motorists to travel through intersections and along residential streets at slower speeds. Curb bulb-outs would be designed to provide ten-foot travel lanes and are typically located leading up to and at the intersection of two roadways, at the intersection of alleys and auto courts (access driveways) with roadways, and along small radius curves. Curb bulb-outs also increase safety for people walking and bicycling as they provide increased visibility and shorter crossing distances of streets while narrowing the streets as vehicles approach the crossing location.

In addition to curb bulb-outs at intersections, on-street parking would be provided along internal roadways within Plat 2F. On-street parking has a measurable effect on vehicle speeds. For many reasons, motorists generally travel at slower speeds in the presence of on-street parking<sup>4</sup>. For example, parked vehicles present the possibility of motorists entering/exiting the flow of traffic which requires more attentive driving behavior and slower speeds. Parked vehicles also give the perception of narrower travel lanes which reduces vehicular speeds.

Within Plat 2F, curb bulb-outs are provided periodically along each roadway between parking spaces including at each intersection. Additionally, curb bulb-outs are also present where the multi-use trail crosses Mountain Ash Avenue SE to provide additional visibility and a shorter distance for pedestrians and bicyclists.

## Summary and Conclusions

- This traffic impact study analyzes the impacts of Plat 2F the Ten Trails MPD. Plat 2F would construct 65 age-qualified housing units, 16 single-family dwelling units, and 42 multi-family dwelling units.
- With consideration to the previously approved land uses of Phase 1A, Phase 1B and Phase 2 of the Ten Trails and Lawson Hills MPDs, a total of 1,346 single-family residential dwelling units, 510 multifamily residential dwelling unit, 262 senior adult detached dwelling units, a 600-student elementary school, 8,000 square feet of office, and 271,000 square feet of retail would be constructed through build-out of Plat 2F (2026). It is estimated these land uses would generate approximately 2,110 net new off-site weekday PM peak hour vehicle trips. This is equivalent to 2,110 ERUs.
- With the development of Plat 2F in addition to the previously approved Phase 1A, Phase 1B and Phase 2 plats, off-site improvements would be triggered at seven intersections. This would include a traffic signal and northbound right turn lane at SE 288th Street/216th Street SE, channelization improvements at SR Covington-Sawyer

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<sup>4</sup> This is supported in (1) Marshall, W., N. Garrick and G. Hansen. "Reassessing On-Street Parking." *Transportation Research Record*, No. 2046 (2008): 45-52 and (2) Morrison, B. "Residential Street Width, On-Street Parking and Accident Frequency." 22nd Conference Proceedings of the Australian Road Research Board, 2006. It is also worth noting that lower-speed streets with on-street parking also have some of the lowest collision rates with respect to serious accidents. Likewise, pedestrian safety is enhanced as on-street parking provides a buffer or barrier between pedestrian traffic and vehicular traffic. Therefore, facilities with on-street parking tend to be safer and more walkable than facilities without on-street parking.



Road/216th Avenue SE, channelization improvements and traffic signals at SR 169/Baker Street and SR 169/Lawson Street, a traffic signal at Roberts Drive/Ten Trails Place SE, a roundabout at Lake Sawyer Road/Ten Trails Parkway SE, and a traffic signal at SR 169/SE 288th Street.

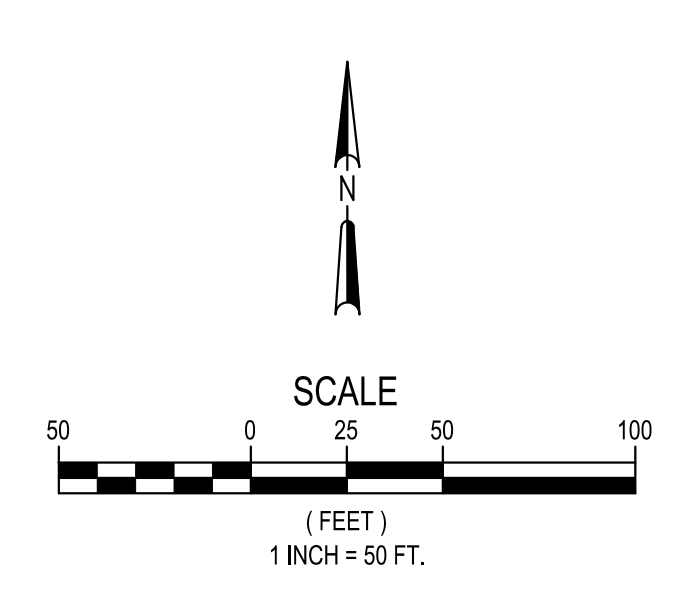
- Historical collision data at the study intersections and roadways was reviewed for the most recent (non-pandemic) three-year period. The SE Kent-Kangley Road / Landsburg Road SE intersection had the highest rate of collisions for the off-site study intersections with 1.63 collisions per million entering vehicles. All roadways had collision rates less than the average collision rate along roadways in both King County and Washington State.
- Construction of Plat 2F is estimated to begin in July 2023 and continue through May 2026 in separate phases. The vertical construction is anticipated to generate the most weekday daily and PM peak hour trips. During this time, the project is estimated to generate approximately 7 combined trips during the PM peak hour, significantly less than the Plat 2F build-out. As such, no construction mitigation measures are deemed necessary.
- Traffic calming measures would be implemented throughout Plat 2F. This will include providing curb bulb-outs, small radius curves, and on-street parking on the neighborhood roads to encourage lower vehicle speeds.

Attachments 1-3



## **Attachment 1: Conceptual and Preliminary Site Plans**





## 2023-10-24



## **Attachment 2: Trip Generation Calculations**



## 2026 PM Peak Hour Trip Generation – Ten Trails (Plat 2F)

Weekday PM Peak Hour - Plat 2F					Gross Trips <sup>1</sup>				Internal Trips <sup>2</sup>				Pass-By Trips <sup>3</sup>				Net Trips <sup>4</sup>		
Land Use	ITE LU <sup>5</sup>	Size <sup>6</sup>	Ave or Eqn	Trip Rate <sup>7</sup>	Total Trips	Inbound <sup>8</sup>	In	Out	Rate	In	Out	Total	Rate	Total	In	Out	Total	In	Out
Single Family	210	16 DUs	Eqn	1.11	18	63%	11	7	0%	0	0	0	0%	0	0	0	18	11	7
Multifamily	221	42 DUs	Eqn	0.92	39	63%	25	14	0%	0	0	0	0%	0	0	0	39	25	14
Senior Adult Housing	251	65 DUs	Eqn	0.49	32	61%	20	12	0%	0	0	0	0%	0	0	0	32	20	12
Elementary School	520	Students	Ave	0.16	0	46%	0	0	30%	0	0	0	0%	0	0	0	0	0	0
Office	710	SF	Eqn	0.00	0	17%	0	0	0%	0	0	0	0%	0	0	0	0	0	0
Retail	820	SF	Eqn	0.00	0	48%	0	0	0%	0	0	0	20%	0	0	0	0	0	0
<b>Total</b>					<b>89</b>		<b>56</b>	<b>33</b>		<b>0</b>	<b>0</b>	<b>0</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>89</b>	<b>56</b>	<b>33</b>

## 2026 PM Peak Hour Trip Generation – Ten Trails (Phase 1A, 1B and 2 Combined)

Weekday PM Peak Hour - Cumulative Phase 1A, 2 and 1B					Gross Trips <sup>1</sup>				Internal Trips <sup>2</sup>				Pass-By Trips <sup>3</sup>				Net Trips <sup>4</sup>		
Land Use	ITE LU <sup>5</sup>	Size <sup>6</sup>	Ave or Eqn	Trip Rate <sup>7</sup>	Total Trips	Inbound <sup>8</sup>	In	Out		In	Out	Total	Rate	Total	In	Out	Total	In	Out
Single Family	210	1,240 DUs	Eqn	0.85	1059	63%	667	392		138	57	195	0%	0	0	0	864	529	335
Multifamily	221	438 DUs	Eqn	0.48	209	63%	132	77		24	9	33	0%	0	0	0	176	108	68
Senior Adult Housing	251	262 DUs	Eqn	0.36	94	61%	57	37		11	4	15	0%	0	0	0	79	46	33
Elementary School	520	600 Students	Ave	0.16	96	46%	44	52		13	16	29	0%	0	0	0	67	31	36
Office	710	8,000 SF	Eqn	2.55	20	17%	3	17		2	3	5	0%	0	0	0	15	1	14
Retail	820	271,000 SF	Eqn	4.27	1157	48%	555	602		59	158	217	20%	188	94	94	752	402	350
<b>Total</b>					<b>2,635</b>		<b>1,458</b>	<b>1177</b>		<b>247</b>	<b>247</b>	<b>494</b>		<b>188</b>	<b>94</b>	<b>94</b>	<b>1,953</b>	<b>1117</b>	<b>836</b>

## 2026 PM Peak Hour Trip Generation – Lawson Hills

Weekday PM Peak Hour - Phase 2					Gross Trips <sup>1</sup>				Internal Trips <sup>2</sup>				Pass-By Trips <sup>3</sup>				Net Trips <sup>4</sup>		
Land Use	ITE LU <sup>5</sup>	Size <sup>6</sup>	Ave or Eqn	Trip Rate <sup>7</sup>	Total Trips	Inbound <sup>8</sup>	In	Out		In	Out	Total	Rate	Total	In	Out	Total	In	Out
Single Family	210	106 DUs	Eqn	0.99	105	63%	66	39		0	0	0	0%	0	0	0	105	66	39
Multifamily	221	72 DUs	Eqn	0.72	52	63%	33	19		0	0	0	0%	0	0	0	52	33	19
Senior Adult Housing	251	DUs	Eqn	0.00	0	61%	0	0		0	0	0	0%	0	0	0	0	0	0
Elementary School	520	Students	Ave	0.16	0	46%	0	0		0	0	0	0%	0	0	0	0	0	0
Office	710	SF	Eqn	0.00	0	17%	0	0		0	0	0	0%	0	0	0	0	0	0
Retail	820	SF	Eqn	0.00	0	48%	0	0		0	0	0	20%	0	0	0	0	0	0
<b>Total</b>					<b>157</b>		<b>99</b>	<b>58</b>		<b>0</b>	<b>0</b>	<b>0</b>		<b>0</b>	<b>0</b>	<b>0</b>	<b>157</b>	<b>99</b>	<b>58</b>

Note: DU = dwelling unit, SF= square feet

1. Total vehicle trips generated by the proposed land uses during the weekday PM peak hour based on rates from Institute of Transportation Engineers (ITE) Trip Generation Manual (11th Edition, 2021).

2. Trips that are anticipated to remain internal to the proposed land uses and would not use roads external to the MPD calculated using the methodology and rates from ITE Trip Generation Handbook (3rd Edition, 2017).

3. Vehicle trips that would already be on the adjacent street system and would make an intermediate stop at the proposed land uses before continuing to their final destination based on rates from Trip Generation Handbook.

4. The overall new vehicle trip to the street system anticipated to be generated by the proposed land uses which would travel externally to the proposed land uses.

5. Land use code used in the ITE Trip Generation Manual.

6. Proposed land use size.

7. Trip generation rate based on either the average trip rate or regression equation from the Trip Generation Manual.

8. Percentage of trips travelling into the development during the weekday PM peak hour based on rates from the Trip Generation Manual.

### **Attachment 3: Construction Traffic Memorandum**







**DAVID EVANS  
AND ASSOCIATES INC.**

September 6, 2023

Andy Williamson, MDRT/Economic Development Director  
City of Black Diamond – P.O. Box 599  
24301 Roberts Drive  
Black Diamond, WA 98010-0559

**SUBJECT: Ten Trails Phase 2 Plat F Construction Trips**

Dear Mr. Williamson,

At the request of CCD Black Diamond Partners LLC we have investigated opportunities for reducing the amount of construction traffic associated with Ten Trails Phase 2 Plat F.

The Villages MPD Condition of Approval No. 110 (set forth in Exhibit C of The Villages MPD Development Agreement on page 23 of 29) states: “Prior to approval of the first implementing plat or site development permit within a phase, the applicant shall submit an overall grading plan that will balance the cut or fill so that the amount of cut or fill does not exceed the other by more than 20%.” In compliance with this condition, Triad prepared and submitted a memorandum “Phase 2 Overall Grading Plan” dated November 21, 2013. We were tasked with coming up with implementable strategies to further reduce construction trips.

In reviewing Phase 2 Plat F’s site grades and typical construction methods, DEA is confident the following additional construction techniques and conditions will further limit construction traffic on the off-site adjoining street network:

- Screen Phase 2 Plat F’s strippings onsite to obtain topsoil for re-use onsite.
- Rocks obtained through the screening of topsoil on Phase 2 Plat F are to be used as fill or crushed for use as base material onsite.
- Sticks obtained through the screening of topsoil on Phase 2 Plat F are to be “chipped” and used for soft surface trails or erosion protection onsite.
- Stockpile any excess material generated through construction of Phase 2 Plat F within the Ten Trails MPD site for use on future phases.

Based on implementation of these strategies, DEA has estimated construction truck and vehicle trips associated with the build out of Phase 2 Plat F. The trip generation estimates have been broken down between crew trips and truck delivery trips as well as early clear & grade, earthwork, In-Plat Road & Utility and vertical construction (buildings). The attached spreadsheets show how assumptions and calculations were used to generate trip estimates for Phase 2 Plat F. And, the attached graphs show the estimated trip distribution along the proposed construction timeline for Phase 2 Plat F.



Early clear & grade construction crews are estimated to generate 960 round trips. Early clear and grade construction truck trips are estimated to generate 30 round trips. Early clear & grade construction is anticipated to occur over 4 months from July through October 2023. Earthwork construction crews are estimated to generate 960 round trips. Earthwork truck deliveries/import/export are estimated to generate 1240 round trips. Earthwork construction is anticipated to occur over 4 months from April through July 2024. Road and utility construction for Phase 2 Plat F is anticipated to occur over 6 months between April 2024 and September 2024. Road and utility (In-Plat) construction crews are estimated to generate 1440 round trips. Road and utility (In-Plat) truck deliveries are estimated to generate 398 round trips. Vertical (building) construction crews were estimated to generate 12,920 round trips. Vertical construction truck deliveries were estimated to generate 861 round trips. Vertical construction is currently anticipated to begin in December of 2024 and continue through May of 2026.

These average crew and truck trips have been graphed based on average trips per day as well as by average PM peak hour trips (*see attached graphs*).

Please let me know if you have any follow-up questions regarding these construction trip generation calculations.

Sincerely,

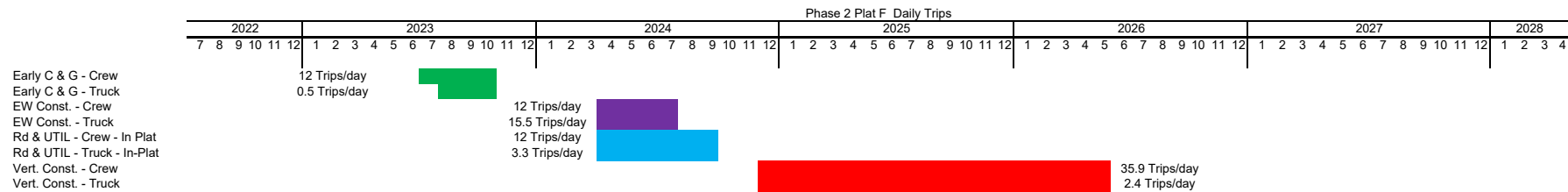
**DAVID EVANS AND ASSOCIATES, INC.**

Thomas P. Matt, P.E.  
Senior Project Engineer

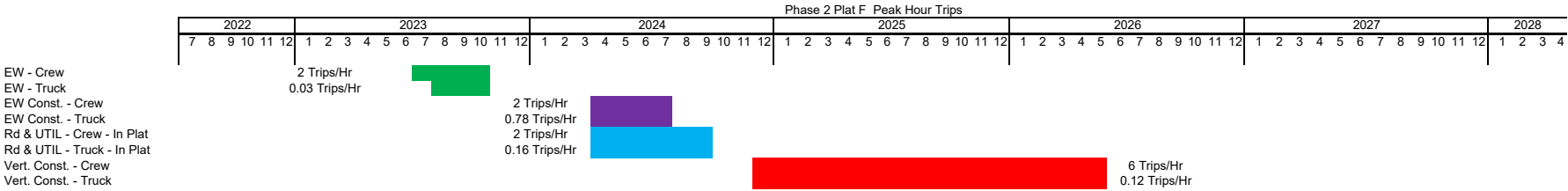


9/6/23

Copies: Justin Wortman  
Attachments/Enclosures: Trip calcs & graphs  
Project Number: OAKPCBDP6001  
File Path: Document1







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**Crew Trip Estimate for Construction of Ten Trails Phase 2 Plat F**

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**Earthwork Construction Crew Trips - Early C & G**

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Earthwork =	4 months, 20 work days per month, 12 crew trucks per day average
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Early C & G Crew Trips =	(4 months)*(20 work days/month)*(12 crew trucks/day) =	960 Trips
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**Earthwork Construction Crew Trips**

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Earthwork =	4 months, 20 work days per month, 12 crew trucks per day average
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Earthwork Crew Trips =	(4 months)*(20 work days/month)*(12 crew trucks/day) =	960 Trips
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Road and Utilities =	6 months, 20 work days per month, 12 crew trucks per day average
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Road and Utility Crew Trips =	(6 months)*(20 work days/month)*(12 crew trucks/day) =	1,440 Trips
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**Vertical Construction Crew Trips**

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Single Family Residence - Detached (SFR-D) construction - assumed 3 months, 20 work days per month, 2 crew trucks per day

SFR-D = 120 trips per SFR-D

Single Family Residential - Attached (SFR-A) assumed at 2/3 of SFR-D, SFR-A = 80 trips per unit

Multi-Family Residential - (MF) assumed at 2/3 of SFR-D, MF = 80 trips per unit

**Vertical Construction Crew Trips**

---

	Units	Trips EA.	Trips
77 SFR-D	77	120	9,240
4 SFR-A	4	80	320
42 MF	42	80	3,360

Subtotal	12,920 Trips
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**Construction Crew Trip Summary**

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Early C & G Crew Trips	960
Earthwork Crew trips	960
Road & Utility Crew Trips In-Plat	1,440
Vertical Crew Trips Res.	12,920
	16,280 Total Crew Trips

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**Truck Trip Estimate for Ten Trails Phase 2 Plat F**

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Logging truck trips have been estimated at 60

Subtotal	60 Trips
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Assume half with early C & G and remainder with earthwork construction

---

**Import Earthwork Balance**

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Estimated Import for earthwork balance

24,000 CY

Assuming 20 CY / Trip

Subtotal	1,200 Trips
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*Import for Wall Construction During Earthwork*

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Assumed 1 delivery truck per 300 Sq Ft of block wall face

Approx. 2,800 Sq Ft/300 Sq Ft = 10 Trips

Subtotal	10 Trips
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Total Trips for earthwork phase

Total	1,270 Trips
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*Import for Road & Alley Construction - In-Plat*

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	LF of Street	Avg Width	Avg Depth	CY	CY/Load	Trips
Ten Trails Parkway	1060	30	0.83	978	20	49
Ten Trails Parkway Sidewalks	1060	8	0.33	104	10	10
Mountain Ash & Emerald	1780	27	0.50	890	20	45
Primrose, Mt Fremont, Emerald Ridge	1800	20	0.67	893	10	89
Driveways A - H	600	20	0.50	222	20	11
Sidewalks	3600	10	0.42	560	10	56
					Total =	260

Note: Assumed outwash material from on Ten Trails site meets requirements for gravel base.

---

*Import for Road & Utility Construction - In-Plat*

---

*Other Import Items as Noted*

Stripping the site assumed at 9" of stripping depth

(20 acres)\*(43,560 sf/acre)\*(0.75 ft)/27 = 24,200 CY

1/2 of stripping volume assumed to be sticks and rocks

Sticks to be chipped on site for use on trails

Rock assumed to be placed as fill or crushed and used on site

1/2 of stripping volume assumed to be topsoil that can be used on-site with 20% import volume for mixing

Topsoil Import = (.5)\*(24,200 CY)\*(0.2) = 2,420 CY

Topsoil Import Trips = 2,420 CY/(25 CY/trip) = 97 Trips

Utility Trips - Deliveries of Pipe, Structures, and Bedding

Assume 1 truck trip per 100' of road and Autocourt construction

Alleys and driveways assumed at half of road construction

Trips = ((1,060LF+ 1,780LF)/100LF/trip) + .5(1,800LF + 600LF)/100LF/trip) = 41 Trips

Subtotal	138 Trips
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### Vertical Construction Delivery Truck Trips for Construction of Ten Trails Phase 2 Plat F

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Single Family Residential - Detached (SFR-D) Construction = 8 Trips per unit  
Single Family Residential - Attached (SFR-A) assumed at 2/3 of SFR-D  
Mult-Family Residential - assumed at 2/3 of SFR-D

$$(77 \text{ SFR-D})(8 \text{ trips}) + (2/3)(46 \text{ SFR-A})(8 \text{ trips}) = 861 \text{ Trips}$$

#### Truck Trip Summary

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Early C & G Truck Trips	30
Earthwork Truck Trips	1,240
Road & Utility Truck Trips In-Plat	398
Vertical Construction Truck Trips	861
	<div>2,530 Total Truck Trips</div>

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Earthwork Construction - Early C & G  
Earthwork construction to occur over 4 months, 20 weekdays/month  
(estimated to occur July 2023 through October 2023)

Crew Vehicle Trips total 960  
 $960/(4*20) = 12 \text{ Trips/day}$

Allowable Work Hours 7AM to 7PM  
Assume 1/3 leave before 4PM  
Assume 1/3 leave after 6PM  
Assume 1/3 leaving between 4PM and 6PM  $12/3 = 4 \text{ trips}$   
Hourly Peak Crew Trips =  $4/2\text{hrs} = 2 \text{ trips/hr}$

Truck Trips  
For distribution of EW truck trips we assumed over 3 month period to allow site prep time  
 $30/(3*20) = 0.5 \text{ trips/day}$   
Deliveries occur throughout the work day 7AM to 7PM  
Assume 10% of truck trips occur between 4PM and 6PM  $(0.5)(0.1) = 0.05 \text{ trips}$   
Hourly Peak Truck Trips =  $0.05 \text{ trips}/2\text{hrs} = 0.03 \text{ trips/hr}$

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Earthwork Construction  
Earthwork construction to occur over 4 months, 20 weekdays/month  
(estimated to occur April 2024 through July 2024)

Crew Vehicle Trips total 960  
 $960/(4*20) = 12 \text{ Trips/day}$

Allowable Work Hours 7AM to 7PM  
Assume 1/3 leave before 4PM  
Assume 1/3 leave after 6PM  
Assume 1/3 leaving between 4PM and 6PM  $12/3 = 4 \text{ trips}$   
Hourly Peak Crew Trips =  $4/2\text{hrs} = 2 \text{ trips/hr}$

Truck Trips  
 $1240/(4*20) = 15.5 \text{ trips/day}$   
Deliveries occur throughout the work day 7AM to 7PM

Assume 10% of truck trips occur between 4PM and 6PM  $(15.5)(0.1) = 1.55$  trips  
Hourly Peak Truck Trips =  $1.55 \text{ trips}/2\text{hrs} = 0.78 \text{ trips/hr}$

---

Road and Utility Construction - In Plat  
Road and Utility construction to occur over 6 months, 20 weekdays/month  
(estimated to occur April through September 2024)

Crew Vehicle Trips total 1440  
 $1440/(6*20) = 12 \text{ Trips/day}$

Allowable Work Hours 7AM to 7PM  
Assume 1/3 leave before 4PM  
Assume 1/3 leave after 6PM  
Assume 1/3 leaving between 4PM and 6PM  $12/3 = 4$  trips  
Hourly Peak Crew Trips =  $4/2\text{hrs} = 2 \text{ trips/hr}$

Truck Trips  
 $398/(6*20) = 3.3 \text{ trips/day}$   
Deliveries occur throughout the work day 7AM to 7PM  
Assume 10% of truck trips occur between 4PM and 6PM  $(3.3)(0.1) = 0.33$  trips  
Hourly Peak Truck Trips =  $0.33 \text{ trips}/2\text{hrs} = 0.16 \text{ trips/hr}$

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Vertical Construction  
Vertical Construction to occur over 18 months, 20 weekdays/month  
(estimated to occur December 2024 through May 2026)

Crew Vehicle Trips total: 12,920  
crew trips total/ $(18*20) = 35.9 \text{ trips/day}$

Allowable Work Hours 7AM to 7PM  
Assume 1/3 leave before 4PM  
Assume 1/3 leave after 6PM  
Assume 1/3 leaving between 4PM and 6PM  $(\text{trips}/\text{day}/3) = 12.0 \text{ trips}$   
Hourly Peak Crew Trips =  $\text{trips}/2\text{hrs} = 6.0 \text{ trips/hr}$

Truck Trips: 861  
 $\text{trips}/(18*20) = 2.4 \text{ trips/day}$   
Delivers occur throughout the work day 7AM to 7PM  
Assume 10% of truck trips occur between 4PM and 6PM =  $0.24 \text{ trips}$   
Hourly Peak Truck Trips =  $\text{trips}/2 \text{ hrs} = 0.12 \text{ trips/hr}$

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Note the calculated trips were conservatively assumed to all occur on weekdays, Monday through Friday



**TEN TRAILS MPD**

**PHASE 2 PLAT F PRELIMINARY PLAT**

**PLN23-0019**

**EXHIBIT 5**

**TITLE:** Phase 2 Traffic Monitoring Plan Approval

**PREPARED BY:** The City of Black Diamond

**DATE:** June 9, 2014



# CITY OF BLACK DIAMOND

---

Physical Address: 24301 Roberts Drive  
Mailing Address: PO Box 599  
Black Diamond, WA 98010

Phone: (360) 886-5700  
Fax: (360) 886-2592  
[www.ci.blackdiamond.wa.us](http://www.ci.blackdiamond.wa.us)

## MEMORANDUM

Date: June 9, 2014

To: Files PLN11-0008, PLN11-0009, PLN11-0010, PLN11-0011, PLN13-0027 & PLN13-0028

From: Andrew Williamson

Re: Approval of Traffic Monitoring Plan for The Villages & Lawson Hills MPDs Phase 2

Pursuant to Section 11.4 (Phasing and Construction of Off-Site Regional Infrastructure Improvements) and Exhibit F (Traffic Monitoring Plan) of The Villages and Lawson Hills Master Planned Development Development Agreements (adopted by Ordinance No. 11-970 & Ordinance No. 11-971), the Master Developer submitted documents pertaining to the Traffic Monitoring Plan. These documents have been reviewed by the Designated Official and by the contracted Master Development Review Team (MDRT) transportation consultant, Parametrix.

The undersigned Designated Official hereby approves the Traffic Monitoring Report prepared for The Villages & Lawson Hills MPDs Phase 2 based on the recommendation of the MDRT transportation consultant.

Andrew Williamson, Designated Official  
Public Works and Transportation



**TEN TRAILS MPD**

**PHASE 2 PLAT F PRELIMINARY PLAT**

**PLN23-0019**

**EXHIBIT 6**

**TITLE:** Ten Trails MPD Phase 2 Detailed Implementation  
Schedule for Regional Infrastructure Improvements Approval

**PREPARED BY:** The City of Black Diamond

**DATE:** June 13, 2014



## CITY OF BLACK DIAMOND

Physical Address: 24301 Roberts Drive  
Mailing Address: PO Box 599  
Black Diamond, WA 98010

Phone: (360) 886-5700  
Fax: (360) 886-2592  
[www.ci.blackdiamond.wa.us](http://www.ci.blackdiamond.wa.us)

### MEMORANDUM

Date: June 13, 2014

To: Files PLN11-0008, PLN11-0009, PLN11-0010, PLN11-0011, PLN13-0027 & PLN13-0028

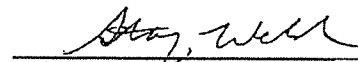
From: Andy Williamson, Stacey Welsh

Re: Approval of Regional Facilities Implementation Schedule for Phase 2, The Villages & Lawson Hills MPDs

Pursuant to Conditions of Approval Nos. 29 and 164 of The Villages MPD approval (Ordinance 10-946) and Sections 11.2 and 11.4.A of The Villages Master Planned Development Development Agreement (adopted by Ordinance 11-970), and Conditions of Approval Nos. 4 and 162 of the Lawson Villages MPD approval (Ordinance 10-947) and Sections 11.2 and 11.4.A of the Lawson Hills Master Planned Development Development Agreement (adopted by Ordinance 11-971) the Master Developer submitted a "Detailed Implementation Schedule – Phase 2 Regional Infrastructure Improvements (January 28, 2014)". This document has been reviewed by the Designated Officials and by the contracted Master Development Review Team (MDRT) civil engineering (RH2) and transportation (Parametrix) consultants.

The undersigned Designated Officials hereby approve the implementation schedule for Phase 2 based on the recommendations of the MDRT civil engineering and transportation consultants.

  
Andrew Williamson, Designated Official

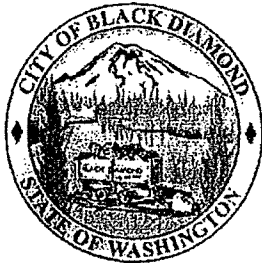
  
Stacey Welsh, Designated Official

29.

Detailed Implementation Schedule of Regional  
Facilities, Triad Associates

January 28, 2014





## REVISION/CORRECTION SUBMITTAL FORM

### Submittal Requirements:

All revisions / correction submittals **MUST** contain the following:

1. A completed City of Black Diamond Revision/Correction submittal form
2. Two (2) sets of revised and/or corrected drawings/sheets (wet stamped by architect, if applicable).
3. Revised structural calculations, if applicable (must be stamped by engineer)
4. A written letter to the City that shows an itemized summary of your submittal (must include sheet and detail numbers)
5. All changes **MUST BE CLOUDED** or **HIGHLIGHTED** on each plan set

Date: 1/31/14

Permit #: PN13-0027 (2C)

Property Address: See the Phase 2C project narrative / plot drawings.  
Project Name: The Villages mPD Phase 2 Prelim. Plat C.  
Contact Person: Colin Lund  
Phone: (425) 898-2100  
Email: clund@yarrowbayholdings.com

### TYPE OF SUBMITTAL:

(X) **SUPPLEMENTAL INFO.**

( ) **REVISION:** A change the applicant has made to a plan that is either:

1. An approved plan already issued by the City or
2. A project under current plan review

( ) **CORRECTION:** An applicant response to a correction letter written by the City to the applicant

Permit Issued? ( ) Yes (X) No \*A plan check fee for revision is \$84 per hour with a minimum of \$42 for ½ hour

Please describe revision/correction submittal:

Detailed Implementation Schedule - Phase 2 Regional Infrastructure  
Improvements dated January 28, 2014.

**Sheets Affected:** \_\_\_\_\_ If more than two (2) sheets will be changed, please submit two (2) new full sets of plans. Revisions on issued permits only require submittal of the affected sheets.

### For City Use Only:

REQ'D APPROVAL	CHECKED BY	ROUTE TO	DATE	INITIAL	COMMENTS	FEES
( )	1. BUILDING					
( )	2. PLANNING	<u>SW</u>	<u>1/30</u>			
( )	3. FIRE					
( )	4. PW	<u>AW</u>	<u>1/30</u>			
( )	5					

TOTAL \$

JAN 10 2014  
RECEIVED

**Detailed Implementation Schedule**  
**Phase 2 Regional Infrastructure Improvements**  
(January 28, 2014)

Pursuant to Conditions of Approval of The Villages MPD (Nos. 29 and 164) and Lawson Hills MPD (Nos. 27 and 169) and consistent with Sections 11.2 – 11.5 of The Villages and Lawson Hills MPD Development Agreements, the following provides a list of both on-site and off-site regional infrastructure and its timing necessary to serve Phase 2 of The Villages and Lawson Hills MPDs. Acceptable engineering alternatives or equivalents may be proposed by the Master Developer during final engineering for the following list of on-site and off-site regional infrastructure improvements as provided in Section 11 of The Villages and Lawson Hills MPD Development Agreements. Model home permits shall be excluded from associated triggers noted below as set forth in Section 11.4(A) of The Villages and Lawson Hills MPD Development Agreements.

**1. Extend The Villages MPD Community Connector (Villages Parkway SE)**

**Construction Threshold**

The extension of The Villages MPD Community Connector through Phase 2 in The Villages MPD will be constructed in phases as necessary to provide access to each Phase 2 plat that takes access from the Community Connector. Each phase of construction will be completed through the intersection of the street that provides access to each Phase 2 plat within The Villages MPD.

**Improvement Details**

The Villages MPD Community Connector (Villages Parkway SE) will be extended in several phases from the end of construction in Phase 1A, near the intersection of Villages Parkway SE and SE Dogwood Street. The Phase 2 extension of the Community Connector will ultimately include construction of approximately 2,800 linear feet of roadway. This section of the Community Connector will consist of 31 feet of Right-of-Way from back of curb to back of curb. The extension of the Community Connector will consist of a single 10 foot travel lane in each direction with 5 foot bike lanes and vertical curbs with gutters. Meandering paths / walkways will be installed on either side of the roadway instead of sidewalks in open space tracts adjacent to the Right-of-Way.

A stormwater line will be constructed within the 2,800 feet of fully constructed road extension to convey runoff from the road improvement and future development within Phase 2 of The Villages MPD to The Villages MPD Parcel D Stormwater Facility. Two water mains (750 and 850 pressure zones) will be installed within the roadway along with gravity sewer to serve portions of Phase 2 of The Villages MPD. The Master Developer may elect to install a purple pipe system within the Community Connector for future use in the event that water for reuse is available.

**2. The Villages MPD Neighborhood Street from Community Connector to Interim Lift Station**

**Construction Threshold**

The location of the interim lift station will be near the intersection of a neighborhood street (along the southeast side of the school site that is shown on The Villages MPD Phase 1A Preliminary Plat, in Division 11L) and the Community Connector (Villages Parkway SE). This neighborhood street with bike lanes will be constructed in phases as necessary to serve development within Phase 2 of The Villages MPD. This road will be constructed to the plat entrance to Phase 2 – Plat A with construction of Phase 2 – Plat A. Right-of-Way will be dedicated for the remainder of this road to the south property line of Parcel D with recording of the Phase 2 – Plat A final plat. The remainder of this road will be constructed with development south of Parcel D or construction of the ultimate lift station.

#### Improvement Details

This neighborhood street will consist of a 64 foot Right-of-Way. The street section will consist of a 10 foot travel lane, 5 foot bike lane, 7 foot parking lane, vertical curb and gutter, 4.5 foot planter strip and a 5 foot sidewalk on each side. The parking lanes may be eliminated to allow for construction of bioretention cells between the bike lane and sidewalk. Utilities will be installed with construction of this neighborhood street to serve Phase 2 and future phases of The Villages MPD.

Stormwater runoff from a portion of the neighborhood street is proposed to be treated within bioretention cells primarily located within the Right-of-Way and infiltrated where infiltratable soils exist. The remainder of the neighborhood street will contain stormwater lines to convey runoff from the street and portions of Phase 2 to the Parcel D stormwater pond within The Villages MPD. This neighborhood street will contain both gravity sewer to serve Phase 2 along with the extension of the Ultimate Sewer Force Main to serve future phases of The Villages MPD. A 750 pressure zone water main will be installed along the length of this street as construction progresses.

### **3. Lawson Parkway**

#### Construction Threshold

Lawson Parkway will be constructed in several phases. One phase will consist of the portion of Lawson Parkway from Lawson Street to the west boundary of Phase 2 – Plat B and will be constructed with Phase 2 – Plat B. Another phase will consist of the portion of Lawson Parkway from Lawson Street to the intersection of Botts Drive to serve the development areas of The Lawson Hills MPD accessed off of Botts Drive. The final phases of construction will complete construction of Lawson Parkway within Phase 2 of the Lawson Hills MPD when required to serve development areas. Each phase of the Lawson Parkway will be constructed or bonded prior to the first plat recorded on the respective side of Lawson Street.

#### Improvement Details

This improvement includes the construction of approximately 2,500 feet of road including utilities. The road section for Lawson Parkway west of Lawson Street will consist of two 10 foot travel lanes, two 5 foot bike lanes, one 7 foot parking lane and one 7 foot sidewalk. A haul road will be rough graded from the west boundary of Phase 2 – Plat B west to SR169. The road section for Lawson Parkway east of Lawson Street will consist of a single 10 foot travel lane and 5 foot bike lane in both directions. The sidewalks will be replaced with a meandering path on either side of the roadway. Stormwater runoff from the road improvements will be conveyed to either the Lawson Hills MPD Hammerhead Stormwater Facility or the Lawson Hills MPD South Stormwater Facility, depending on drainage sub-basin boundary location.

### **4. Frontage Improvements along Lawson Street**

#### Construction Threshold

The frontage improvements along Lawson Street will be constructed in phases with the development of abutting construction phases of the Lawson Hills MPD.

#### Improvement Details

Where the Lawson Hills MPD abuts both sides of Lawson Street, full street improvements will be constructed based on the 2009 City of Black Diamond Engineering Design and Construction Standards and consistent with the City's Comprehensive Plan delineation of Lawson Street as a Minor Arterial. Where the Lawson Hills MPD abuts only one side of Lawson Street, half street frontage improvements will be constructed along the MPD project site in accordance with the 2009 City of Black Diamond Engineering Design and Construction Standards. The half street frontage improvements for a minor



arterial include a 14 to 15 foot wide shared use lane, planter strip of 4 to 6 foot wide, 6 foot wide sidewalk and curb and gutter.

**5. The Villages MPD Parcel D Stormwater Facility**

**Construction Threshold**

The Villages MPD Parcel D Stormwater Facility is required to be constructed in Phase 1A per The Villages Preliminary Plat 1A condition of approval number 53. The Villages Preliminary Plat 1A condition of approval number 53 states "The stormwater facility located to the south of the PPIA and shown on plat sheets RS7 through RS9 dated 8/29/2012 shall be designed and built at this time to accommodate all future phases of The Villages MPD that may potentially drain to it. This condition will be applied during Utility Permit review and approval". See the approved Phase 1A Detailed Implementation Schedule for improvement details.

**6. Lawson Hills MPD South Stormwater Facility**

**Construction Threshold**

The Lawson Hills MPD South Stormwater Facility will be constructed with the first improvements tributary to the facility. The improvements to trigger construction of the Lawson Hills MPD South Stormwater Facility are the lots being constructed south of Lawson Creek and west of Lawson Hills Spine Road. Construction of this stormwater facility may be built in phases to accommodate each improvement tributary to it.

**Improvement Details**

The Lawson Hills MPD South Stormwater Facility will be designed and constructed in accordance with the 2005 Stormwater Management Manual for Western Washington as adopted by the City of Black Diamond and the 2009 City of Black Diamond Engineering Design and Construction Standards.

**7. Lawson Hills MPD Hammerhead Stormwater Facility**

**Construction Threshold**

The Lawson Hills MPD Hammerhead Stormwater Facility will be constructed with the first improvements tributary to the facility, which include Lawson Parkway west of Lawson Street and Phase 2 – Plat B.

**Improvement Details**

The Lawson Hills MPD Hammerhead Stormwater Facility will be designed and constructed in accordance with the 2005 Stormwater Management Manual for Western Washington as adopted by the City of Black Diamond and the 2009 City of Black Diamond Engineering Design and Construction Standards.

**8. Intersection Improvement at Lawson Parkway/Lawson Street**

**Construction Threshold**

The intersection of Lawson Parkway and Lawson Street will be created with the construction of Lawson Parkway. The only improvement necessary at this newly constructed intersection will be stop control for Lawson Parkway per the Traffic Monitoring Report for The Villages and Lawson Hills MPDs – Phase 2 by Transpo dated December 2013, page 17.

#### Improvement Details

Stop control will be provided for Lawson Parkway when this intersection is constructed. Additional intersection improvements may be necessary for later phases of the Lawson Hills MPD and will be analyzed by future traffic reports, but will not be required with Phase 2 construction per the Traffic Monitoring Report for The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 17.

### **9. Intersection Improvement at Roberts Drive/Morgan Street**

#### Construction Threshold

Construction of an intersection improvement at Roberts Drive and Morgan Street will commence prior to the City's issuance of a certificate of occupancy for the 1,393<sup>rd</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before Roberts Drive/Morgan Street would otherwise exceed the City's applicable operations standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 29.

#### Improvement Details

A traffic signal is recommended as the intersection improvement at this location. By installing a traffic signal, this intersection will operate at LOS A through Phase 2 build-out and meet the City's LOS C or better standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 29. This intersection improvement is anticipated to consist of a single eastbound and westbound lane on Roberts Drive and a single northbound and southbound lane on Morgan Street to the south of Roberts Drive.

### **10. Intersection Improvement at SE Kent-Kangley Rd/Landsburg Rd SE**

#### Construction Threshold

Construction of an intersection improvement at SE Kent-Kangley Road and Landsburg Road SE will commence prior to the City's issuance of a certificate of occupancy for the 1,393<sup>rd</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before the southbound approach would otherwise exceed the County's applicable operations standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 30.

#### Improvement Details

The intersection improvement at this location is anticipated to consist of constructing a southbound left-turn lane so that this intersection will operate in the LOS C-D range through Phase 2 build-out and meet King County's LOS E or better standard. Construction of this left-turn lane will commence prior to the City's issuance of a certificate of occupancy for the 1,393<sup>rd</sup> ERU (of Phases 1A and Phase 2 combined) to ensure the improvement is completed before the southbound approach would otherwise exceed the County's applicable operations standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 30.

### **11. Intersection Improvement at SR 169/SE 288<sup>th</sup> Street**

#### Construction Threshold

Construction of an intersection improvement at SR 169 and SE 288<sup>th</sup> Street will commence prior to the City's issuance of a certificate of occupancy for the 1,393<sup>rd</sup> ERU (of Phases 1A and 2 combined). See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, pages 29-30.

#### Improvement Details

The intersection improvement at this location is anticipated to consist of the installation of a traffic signal. By installing a traffic signal, this intersection will operate at LOS A through Phase 2 build-out and meet WSDOT's LOS D or better standard. Per Exhibit F of The Villages and Lawson Hills MPD Development Agreements, engineering design of this traffic signal should commence prior to the City's issuance of a certificate of occupancy for the 1,393<sup>rd</sup> ERU (of Phases 1A and 2 combined). See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, pages 29-30. This intersection improvement is anticipated to have a northbound through lane, a northbound left turn lane, a southbound through lane, a southbound right turn lane and eastbound right and left turn lanes.

### **12. Intersection Improvement at SE 288<sup>th</sup> Street/216<sup>th</sup> Ave SE**

#### Construction Threshold

Construction of an intersection improvement at SE 288<sup>th</sup> Street and 216<sup>th</sup> Ave SE will commence prior to the City's issuance of a certificate of occupancy for the 1,462<sup>nd</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before SE 288<sup>th</sup> Street / 216<sup>th</sup> Avenue would otherwise exceed the City's applicable operations standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 27.

#### Improvement Details

The intersection improvement at this location is anticipated to consist of the installation of a traffic signal. By installing a traffic signal, this intersection will operate at LOS A through Phase 2 build-out and meet the City's LOS C or better standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 27.

This intersection improvement is anticipated to consist of a single northbound lane, a southbound through lane, a southbound left turn lane, and westbound right and left turn lanes.

### **13. Intersection Improvement at SE Auburn-Black Diamond Rd/SE Green Valley Rd**

#### Construction Threshold

Construction of an intersection improvement at SE Auburn-Black Diamond Road and SE Green Valley Road will commence prior to the City's issuance of a certificate of occupancy for the 1,687<sup>th</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before the northbound left-turning movement would otherwise exceed the County's applicable operations standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 32.

#### Improvement Details

The intersection improvement at this location is anticipated to consist of rechannelizing the west leg of the intersection to provide a refuge/merge area for northbound-to-westbound left turning vehicles. By rechannelizing the west leg of the intersection to provide a refuge/merge area, this intersection will operate at LOS C through Phase 2 build-out and meet King County's LOS E or better standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 32.



#### **14. Intersection Improvement at Roberts Drive/Lake Sawyer Rd SE**

##### **Construction Threshold**

Construction of an intersection improvement at Roberts Drive and Lake Sawyer Road SE is only necessary if Villages Parkway SE is not constructed between Roberts Drive and Lake Sawyer Road SE. If Villages Parkway SE is not constructed between Roberts Drive and Lake Sawyer Road SE, construction of an intersection improvement at this location will commence prior to the City's issuance of a certificate of occupancy for the 1,857<sup>th</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before Roberts Drive/Lake Sawyer Road SE would otherwise exceed the City's applicable operations standard. *See* Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 28.

##### **Improvement Details**

The intersection improvement at this location is anticipated to consist of constructing a right-turn slip lane on the northwest corner of the intersection so that this intersection will operate at LOS B and meet the City's LOS C or better standard. This right-turn slip lane will allow southbound-to-westbound traffic to turn right without entering the roundabout. This improvement is necessary only if Villages Parkway SE is not constructed between Roberts Drive and Lake Sawyer Road SE. *See* Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 28.

Regional Infrastructure Improvement	Construction Threshold	Funding Responsibility*
<b>1. Extend The Villages MPD Community Connector</b>	The extension of The Villages MPD Community Connector through Phase 2 in The Villages MPD will be <b>constructed in phases as necessary</b> to provide access to each Phase 2 plat that takes access from the Community Connector. Each phase of construction will be completed through the intersection of the street that provides access to each Phase 2 plat within The Villages MPD.	Master Developer
<b>2. The Villages MPD Neighborhood Street from Community Connector to Interim Lift Station</b>	The location of the interim lift station will be near the intersection of a neighborhood street (along the southeast side of the school site that is shown on The Villages MPD Phase 1A Preliminary Plat, in Division 1L) and the Community Connector (Villages Parkway SE). This neighborhood street with bike lanes will be <b>constructed in phases as necessary</b> to serve development within Phase 2 of The Villages MPD. This road will be constructed to the plat entrance to Phase 2 – Plat A with construction of Phase 2 – Plat A. Right-of-Way will be dedicated for the remainder of this road to the south property line of Parcel D with recording of the Phase 2 – Plat A final plat. The remainder of this road will be constructed with development south of Parcel D or construction of the ultimate lift station.	Master Developer
<b>3. Lawson Parkway</b>	Lawson Parkway will be constructed in several phases. One phase will consist of the portion of Lawson Parkway from Lawson Street to the west boundary of Phase 2 – Plat B and will be constructed with Phase 2 – Plat B. Another phase will consist of the portion of Lawson Parkway from Lawson Street to the intersection of Botts Drive to serve the development areas of The Lawson Hills MPD accessed off of Botts Drive. The final phases of construction will complete construction of Lawson Parkway within Phase 2 of the Lawson Hills MPD when required to serve development areas. Each phase of the Lawson Parkway will be <b>constructed or bonded prior to the first plat recorded on the respective side of Lawson Street.</b>	Master Developer
<b>4. Frontage Improvements along Lawson Street</b>	The frontage improvements along Lawson Street will be <b>constructed in phases with the development of abutting construction phases of the Lawson Hills MPD.</b>	Master Developer
<b>5. The Villages MPD Parcel D Stormwater</b>	The Villages MPD Parcel D Stormwater Facility is required to be constructed in Phase 1A per The Villages Preliminary Plat	Master Developer

<b>Pond and Infiltration Facility</b>	1A condition of approval number 53. The Villages Preliminary Plat 1A condition of approval number 53 states "The stormwater facility located to the south of the PPIA and shown on plat sheets RS7 through RS9 dated 8/29/2012 shall be designed and built at this time to accommodate all future phases of The Villages MPD that may potentially drain to it. This condition will be applied during Utility Permit review and approval".	
<b>6. Lawson Hills MPD South Stormwater Facility</b>	The Lawson Hills MPD South Stormwater Facility will be constructed with the first improvements tributary to the facility. The improvements to trigger construction of the Lawson Hills MPD South Stormwater Facility are the lots being constructed south of Lawson Creek and west of Lawson Hills Spine Road. Construction of this stormwater facility may be built in phases to accommodate each improvement tributary to it.	Master Developer
<b>7. Lawson Hills MPD Hammerhead Stormwater Facility</b>	The Lawson Hills MPD Hammerhead Stormwater Facility will be constructed with the first improvements tributary to the facility, which include Lawson Parkway west of Lawson Street and Phase 2 – Plat B.	Master Developer
<b>8. Intersection Improvement at Lawson Parkway/Lawson Street / Botts Drive</b>	The intersection of Lawson Parkway and Lawson Street will be created with the construction of Lawson Parkway. The only improvement necessary at this newly constructed intersection will be stop control for Lawson Parkway per the Traffic Monitoring Report for The Villages and Lawson Hills MPDs – Phase 2 by Transpo dated December 2013, page 17.	Master Developer
<b>9. Intersection Improvement at Roberts Dr / Morgan St</b>	Construction of an intersection improvement at Roberts Drive and Morgan Street will commence prior to the City's issuance of a certificate of occupancy for the 1,393 <sup>rd</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before Roberts Drive/Morgan Street would otherwise exceed the City's applicable operations standard. See Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 29.	Master Developer
<b>10. Intersection Improvement at SE Kent-Kangley / Landsburg Rd SE</b>	Construction of an intersection improvement at SE Kent-Kangley Road and Landsburg Road SE will commence prior to the City's issuance of a certificate of occupancy for the 1,393 <sup>rd</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before the southbound approach would otherwise exceed the County's applicable operations	Master Developer



	standard. <i>See</i> Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 30.	
<b>11. Intersection Improvement at SR 169/SE 288<sup>th</sup> St</b>	Construction of an intersection improvement at SR 169 and SE 288 <sup>th</sup> Street will commence prior to the City's issuance of a certificate of occupancy for the 1,393 <sup>rd</sup> ERU (of Phases 1A and 2 combined). <i>See</i> Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, pages 29-30.	Master Developer
<b>12. Intersection Improvement at SE 288<sup>th</sup> St/216<sup>th</sup> Ave SE</b>	Construction of an intersection improvement at SE 288 <sup>th</sup> Street and 216 <sup>th</sup> Ave SE will commence prior to the City's issuance of a certificate of occupancy for the 1,462 <sup>nd</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before SE 288 <sup>th</sup> Street / 216 <sup>th</sup> Avenue would otherwise exceed the City's applicable operations standard. <i>See</i> Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 27.	Master Developer
<b>13. Intersection Improvement at SE Auburn-Black Diamond Rd/SE Green Valley Rd</b>	Construction of an intersection improvement at SE Auburn-Black Diamond Road and SE Green Valley Road will commence prior to the City's issuance of a certificate of occupancy for the 1,687 <sup>th</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before the northbound left-turning movement would otherwise exceed the County's applicable operations standard. <i>See</i> Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 32.	Master Developer
<b>14. Intersection Improvement at Roberts Drive/Lake Sawyer Rd SE</b>	Construction of an intersection improvement at Roberts Drive and Lake Sawyer Road SE is only necessary if Villages Parkway SE is not constructed between Roberts Drive and Lake Sawyer Road SE. If Villages Parkway SE is not constructed between Roberts Drive and Lake Sawyer Road SE, construction of an intersection improvement at this location will commence prior to the City's issuance of a certificate of occupancy for the 1,857 <sup>th</sup> ERU (of Phases 1A and 2 combined) to ensure the improvement is completed before Roberts Drive/Lake Sawyer Road SE would otherwise exceed the City's applicable operations standard. <i>See</i> Traffic Monitoring Report, The Villages and Lawson Hills MPDs – Phase 2 by Transpo Group dated December 2013, page 28.	Master Developer

\*Funding Responsibility is subject to the terms of The Villages and Lawson Hills MPD Development Agreements.

**TEN TRAILS MPD**

**PHASE 2 PLAT F PRELIMINARY PLAT**

**PLN23-0019**

**EXHIBIT 7**

**TITLE:** Ten Trails MPD Phase 2 Fiscal Impact Analysis Approval

**PREPARED BY:** The City of Black Diamond

**DATE:** September 21, 2021



# CITY OF BLACK DIAMOND

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## **MEMORANDUM**

**Date: September 21, 2021**

**From: Andrew Williamson**

**Re: Approval Fiscal Impact Analysis for Phase 1A, 2 and 1B**

Pursuant to Section 13.6 (Fiscal Impacts Analysis) of The Villages Master Planned Development Development Agreement (adopted by Ordinance 11-970) the Master Developer submitted documents pertaining to the Fiscal Impacts Analysis. These documents have been reviewed by the Designated Official and the contracted Master Development Review Team (MDRT) fiscal impact consultant, FCS Group.

Mr. Martin Chaw of FCS Group, prepared a memorandum dated September 17, 2021, documenting the independent review of the subject fiscal analysis prepared by DPGF, titled, "Phase 1A & 2 & 1B Fiscal Impact Analysis for the Ten Trails (fka The Villages) and Lawson Hills Master Planned Development dated August 20, 2021. The recommendation is: "Based on our review, we recommend that the City approve DPGF's August 20, 2021 FIA as submitted". This fulfills the requirements of Section 13.6 of the Development Agreement"

The additional fiscal condition #83 set by the Hearing Examiner's condition of approval on the Villages MPD Phase 1A and Phase 2 has been fulfilled.

The undersigned Designated Official hereby approves the Fiscal Analysis prepared for Ten Trails fkaThe Villages MPD Phase 1A and Phase 2 and Phase 1B based on the recommendation of the MDRT fiscal consultant.

**Andrew Williamson**  
**Designated Official Fiscal Analysis**